

2.0 PROJECT DESCRIPTION

The proposed project, known as the Chandler Ranch Area Specific Plan (hereafter referred to as the “Specific Plan”), is a specific plan that gives detailed guidelines for the development of the Chandler Ranch Area. The City’s General Plan requires that a specific plan be adopted for the area before any portion can be developed. The main features of the Specific Plan include hillside and open space areas, single and multi-family residential neighborhoods, trails providing pedestrian and bicycle access to the neighboring Barney Schwartz Park, and areas designated for commercial use.

2.1 PROJECT APPLICANT

City of El Paso de Robles
1000 Spring Street
El Paso de Robles, CA 93446

2.2 PROJECT LOCATION

The 826.7-acre Chandler Ranch site is located at the eastern end of the City of Paso Robles. The subject property is bounded by State Route 46 East on the north, the intersection of Fontana and Linne Roads to the south, Golden Hill Road on the west and the City boundary on the east (Figures 2-1 and 2-2). Barney Schwartz Park is located within the northernmost extent of the study area.

2.3 EXISTING SITE CHARACTERISTICS

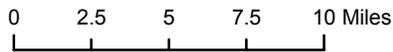
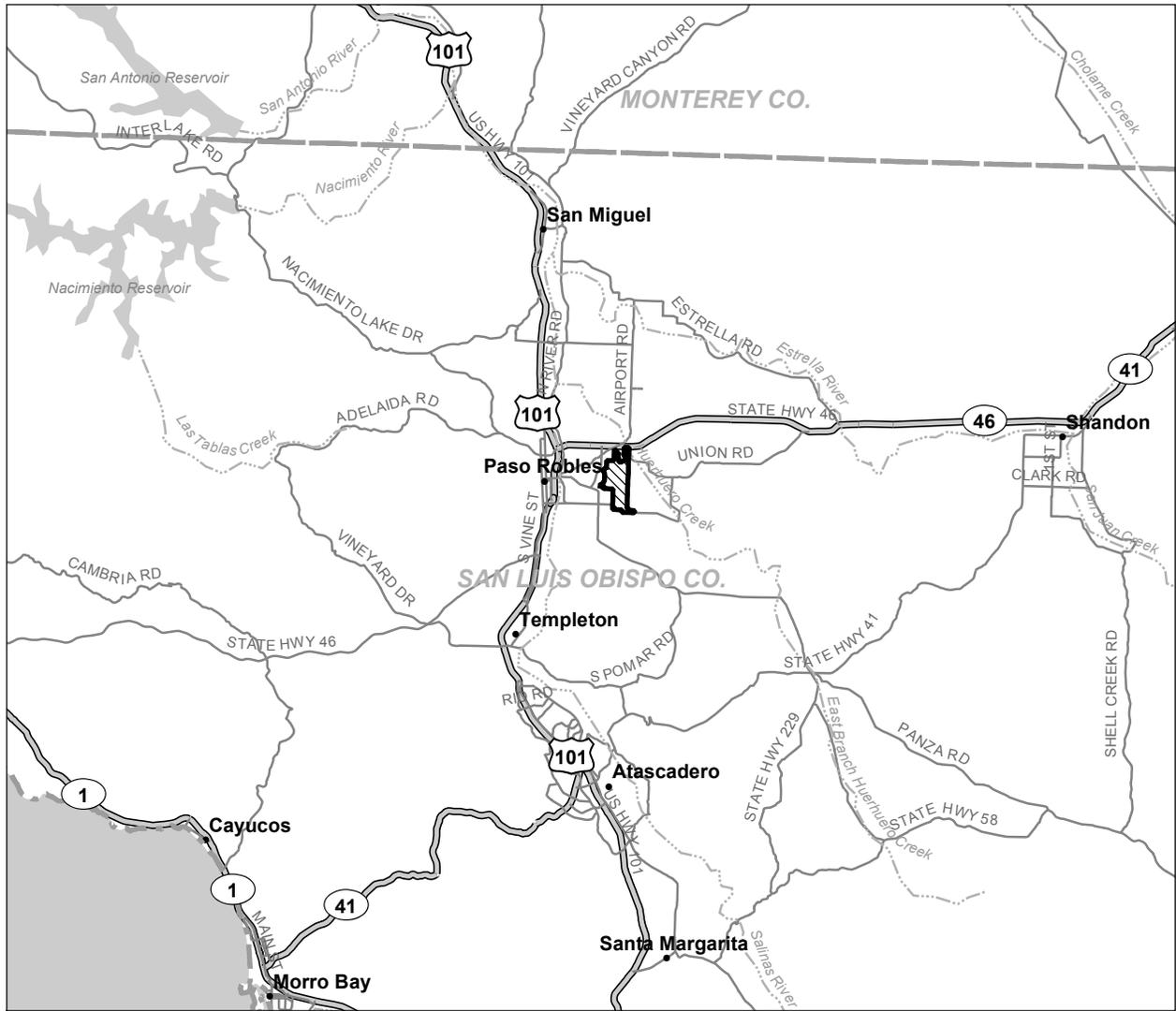
2.3.1 General Site Characteristics

The Chandler Ranch Area can be characterized by rolling terrain with a major south-north trending drainage area in the central portion of the site. Steeper slopes are located along the flanks of the drainage, with the highest portions of the site in the center, within an area characterized by oak woodland. Elevations across the site range from a high of approximately 1,000 feet along the main ridgeline to 732 feet at the northeast corner of the subject property, closest to Huerhuero Creek (Figure 2-1). On-site drainages are intermittent streams which flow north and northeast draining into Huerhuero Creek, which is located in the northeast corner of the Specific Plan area.

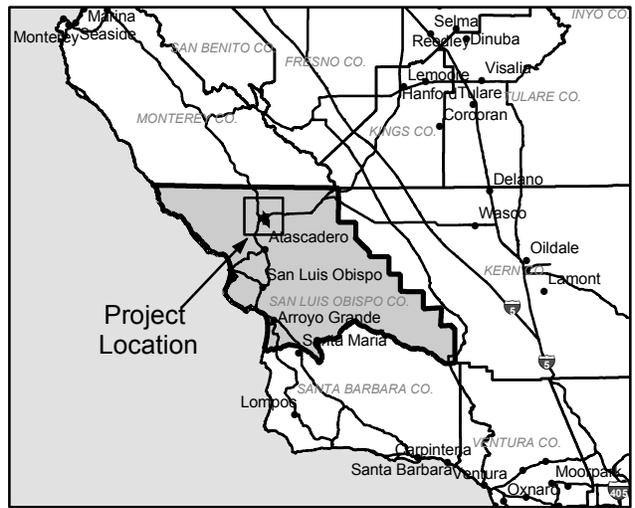
The subject property is currently used for livestock grazing and has historically been used for both dryland farming and grazing. Model homes (some occupied) from a 1960s-era development project are located in the southeast corner of the specific plan, in an **County unincorporated** area called “Our Town.” The historic Chandler Ranch headquarters (house and barn) is located in the southwest corner of the specific plan area, on the Wilcox parcel. The Chandler Ranch site is controlled by seven property owners. Table 2-1 summarizes the ownership characteristics of the properties on the site.



Chandler Ranch Area Specific Plan EIR
Section 2.0 Project Description



 Project Location

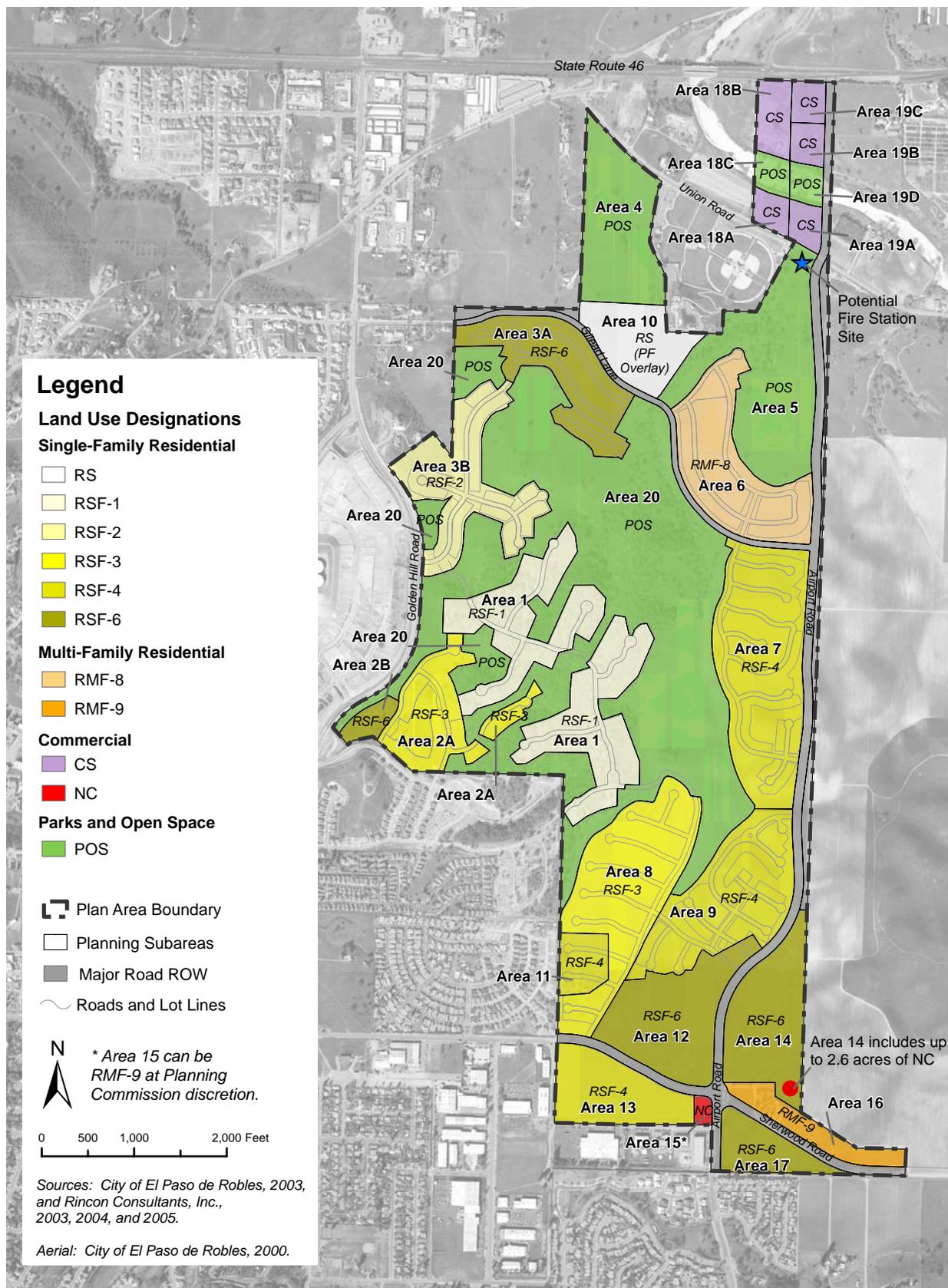


Source: U.S. Bureau of the Census, TIGER Data, 2000.

Regional Location

Figure 2-1





Proposed Specific Plan Land Use

Figure 2-2

Table 2-1. Property Ownership and Acreage Summary

Property	Acreage
Jonatkim Enterprises	672.4
Chandler Sand & Gravel	85.1
Meixner	17.6
Branch	15.3
Our Town (Tract 232) Family Trust (multiple owners)	14.1
Centex Homes *	13.2
Wilcox	7.7
Rupert	1.3
TOTAL	826.7

* Includes about 0.8 acres of Linne Road ROW that would revert to this property owner under the Specific Plan.

2.3.2 Surrounding Land Uses

The Specific Plan area is surrounded by existing developments. The areas north, west and south of the site are within the City of Paso Robles. Specifically, Barney Schwartz Park, and Huerhuero Creek are north of the site, beyond which lies Highway 46 East and areas designated for agriculture, parks/open space, commercial and industrial uses. Various residential and commercial uses are west of the site, particularly west of Golden Hill Road. The southern portion of the site is flanked by additional residential, commercial and industrial land uses.

The area to the east is within an unincorporated portion of San Luis Obispo County. The County recently approved an agricultural cluster subdivision on 851 acres, in which 43 homes would be built. The remaining portion of that area is to be preserved in agricultural use, which at this time is wine grape cultivation.

2.3.3 Existing General Plan Land Use Designations and Zoning

a. General Plan Land Use Designations. Under the adopted 2003 General Plan Update, the Chandler Ranch Specific Plan area is designated with a Specific Plan, not to exceed 1,439 dwelling units total. However, there are also a series of existing land use designations that would be considerably more restricted than the buildout potential. The purpose of the Specific Plan process at this time is to revisit the underlying land use designations in such a way to provide a more coordinated plan to guide future development in the area. This plan must not exceed the parameters allowed by the Specific Plan overlay designation.

The existing General Plan includes the following underlying land use designations within the planning area:

- BP, Business Park (81.0 acres)
- CS, Commercial Service (34.1 acres)
- NC, Neighborhood Commercial
- RS, Residential Suburban [0.4 Dwelling Units/acre] (575.3 acres)
- RSF-2, Residential Single Family [2 Dwelling Units /acre] (89.6 acres)
- RSF-6, Residential Single-Family [6 Dwelling Units / Acre] (13.2 acres, in subarea 17)
- RMF-9, Residential Multi-Family [9 Dwelling Units / Acre] (14.1 acres, in subarea 16)



b. Zoning. The planning area is located within Subarea 8 of the City's Zoning Map. Within the Specific Plan area, there are four City zoning designations, including the following:

- RA, Residential Agriculture [3-acre minimum parcel size]
- R1-B5, Single Family [2-acre minimum parcel size]
- R1-B3, Single family [20,000 square feet minimum parcel size]
- PM, Planned Industrial

2.4 PROJECT CHARACTERISTICS

2.4.1 Specific Plan Legal Requirements

a. Specific Plan Authority. State law authorizes cities and counties with complete general plans to prepare and adopt specific plans (Government Code Sections 65450 et seq.). These plans have developed as a bridge between the local general plan and individual development proposals, and contain both planning policies and regulations. They often combine zoning regulations, capital improvement programs, detailed development standards, and other regulatory schemes into one document which can be tailored to meet the needs of the specific area.

Government Code Section 65451 describes the required contents of a Specific Plan as follows:

(a) A specific plan shall include a text and a diagram or diagrams which specify all of the following in detail:

(1) The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.

(2) The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan.

(3) Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources, where applicable.

(4) A program of implementation measures including regulations, programs, public works projects, and financing measures necessary to carry out paragraphs (1), (2), and (3).

(b) The specific plan shall include a statement of the relationship of the specific plan to the general plan.

The Specific Plan includes the goals, policies, development standards, and implementation measures that would guide future development, in accordance with state law. The Specific Plan's relationship to the City's General Plan is discussed below.



b. Relationship to the General Plan. The Chandler Ranch Area Specific Plan is designed to meet the goals established in the City's General Plan by providing a framework for future development of the Chandler Ranch Expansion Area. The Specific Plan provides a bridge between the City's General Plan and detailed plans for development and will direct all facets of future development within the Chandler Ranch Area including:

- Designation of land uses;
- Designation of required access & circulation elements;
- Location and sizing of infrastructure;
- Phasing of development/thresholds of development;
- Financing methods for public improvements; and
- Establishing standards of development.

2.4.2 Land Use Pattern and Development Potential Under the Specific Plan

The Chandler Ranch Area Specific Plan provides policies and programs that will guide future annexation and development of the area. The Specific Plan calls for open space, residential, and mixed/commercial uses as well as associated roads and pedestrian/bike paths. A potential site for a school is also identified. The Specific Plan would generate an estimated 546 students at various grade levels.

There are twelve land use categories where additional requirements apply. These land use categories guide the general distribution, location and extent of the various types of land uses in the Specific Plan area. Locations of subareas and land use categories are shown in Figure 2-2.

Land Use Categories in the Specific Plan Area include the following, with development areas further subject to a PD (Planned Development) designation:

- Residential Suburban (RS)
- Residential Single-Family-1 (RSF-1)
- Residential Single-Family-2 (RSF-2)
- Residential Single-Family-3 (RSF-3)
- Residential Single-Family-4 (RSF-4)
- Residential Single-Family-6 (RSF-6)
- Residential Multi-Family-8 (RMF-8)
- Residential Multi-Family-9 (RMF-9)
- Neighborhood Commercial (NC)
- Commercial Service (CS)
- Parks and Open Space (POS)
- Public Facility (PF)

Unless otherwise noted in this Specific Plan, development standards associated with these categories are the same as those included in the 2003 General Plan, and as implemented through the Zoning Ordinance.



The following is a brief description of the pattern and intensity of the proposed development pattern within the Chandler Ranch Specific Plan area. The Subareas referred to in this list correspond directly to those shown in Figure 2-2 and Tables 2-2a through 2-2c.

Residential Density and Pattern:

1. Within planned development areas, residential densities would range in density, typically increasing in intensity from north to south. This density pattern is intended to avoid conflicts with operations of the Paso Robles Municipal/Regional Airport and of Barney Schwartz Park.
2. Residential densities and patterns are responsive to onsite natural resources and topography. In general, higher intensity development would occur in more level areas with fewer identified natural resources, such as oak trees and other sensitive habitats.

Commercial Intensity and Pattern:

3. Limited Neighborhood Commercial development could occur in the southern end of the site, near the intersection of Airport Road and the future extension of Sherwood Road.
4. Various commercial service and commercial recreation uses could occur in the vicinity of Barney Schwartz Park. Development intensities would be limited to minimize potential impacts to area roadways, including State Route 46 East, Union Road, and Golden Hill Road.

Circulation:

5. Airport Road would generally follow the eastern boundary of the Specific Plan area.
6. Sherwood Road would be extended into the southern portion of the Specific Plan Area as a major arterial, ultimately allowing for an abandonment of a portion of Linne Road.
7. Gilead Lane would be extended as the primary east-west collector street through the Specific Plan area

Open Space and Trails:

8. On-site drainages would generally be preserved in open space. A total of 304 acres (36% of the site) are preserved as designated open space, most of which would be either in the central portion of the site, with some along the drainage of Huerhuero Creek.
9. The project would include a trail system connecting Barney Schwartz Park with areas to the south of Linne Road, including a potential school site in Subarea 10, via a cohesive central open space system.

Schools:

10. A 13-acre school site could be accommodated in 18.2-acre Subarea 10, in the northern portion of the site, near Barney Schwartz Park. This area is outside of the airport safety zones identified as part of the Airport Land Use Plan.



Table 2-2a. Specific Plan Land Use Designations and Buildout Summary

Subarea	Acres	Land Use Designations	Comments on Land Use	Maximum Dwellings ⁸	Maximum Floor Area	2005 Ownership
By Subarea						
1	64.0	RSF-1	Custom homes	50	-	Jonatkim Enterprises ¹
2a	226.9	RSF-3	Semi-custom homes	37	-	Jonatkim Enterprises ¹
2b	4.0	RSF-6	Townhomes	24	-	Jonatkim Enterprises ¹
3a	29.5	RSF-6	3-pack housing	138	-	Jonatkim Enterprises ¹
3b	30.0	RSF-2	Semi-custom homes	50	-	Jonatkim Enterprises ¹
4	30.0	POS	w/ Recreational Facility	-	-	Jonatkim Enterprises ¹
5	46.5	POS	w/ 2-ac Fire Station	-	-	Jonatkim Enterprises ¹
6	33.7	RMF-8	6-pack housing	222	-	Jonatkim Enterprises ¹
7	54.5	RSF-4	Production housing	141	-	Jonatkim Enterprises ¹
8	46.2	RSF-3	Production housing	100	-	Jonatkim Enterprises ¹
9	42.3	RSF-4	Production housing	95	-	Jonatkim Enterprises ¹
10	18.2	RS (PF overlay)	School/re-vegetated area	note 2	-	Jonatkim Enterprises
11	7.7	RSF-4	Production housing	31	-	Wilcox
12	30.6	RSF-6	Mixed housing	122	-	Chandler Sand & Gravel
13	20.6	RSF-4	Mixed housing	82	-	Chandler Sand & Gravel
14	25.2	RSF-6 / NC ³	Housing/Neigh Com	150	19,000	Chandler Sand & Gravel
15	0.7	RMF-9 or NC ⁴	Housing or Comm	note 4	14,000	Rupert
16	12.3	RMF-9	Townhomes	139	-	Our Town
17	9.0	RSF-6	Production housing	90	-	Centex Homes
18a	4.0	CS	Retail/office	-	46,000	Branch
18b	7.0	CS	Retail/office	-	104,000	Branch
18c	4.0	POS	Huerhuero Creek	-	-	Branch
19a	3.1	CS	Commercial	-	20,000	Meixner
19b	3.5	CS	Commercial	-	62,500	Meixner
19c	3.4	CS	Commercial	-	15,000	Meixner
19d	3.1	POS	Huerhuero Creek	-	-	Meixner
20	220.3	POS	-	-	-	Jonatkim Enterprises
ROW	21.0	-	Airport Road	-	-	Jonatkim Enterprises
ROW	5.0	-	Airport Road	-	-	Chandler Sand & Gravel
ROW	0.3	-	Airport Road	-	-	Rupert
ROW	1.6	-	Airport Road	-	-	Centex Homes
ROW	4.2	-	Airport Road	-	-	Meixner
ROW	9.3	-	Gilead Lane	-	-	Jonatkim Enterprises
ROW	3.7	-	Sherwood Road	-	-	Chandler Sand & Gravel
ROW	0.3	-	Sherwood Road	-	-	Rupert
ROW	1.8	-	Sherwood Road	-	-	Our Town
ROW	2.6	-	Sherwood Road	-	-	Centex Homes
ROW	0.3	-	Union Road	-	-	Branch
ROW	0.3	-	Union Road	-	-	Meixner
Summary and Total						
Residential	429.9	-	-	1,439	-	
Commercial	24.3	-	-	-	280,500	
Public Facility	18.2	-	-	-	-	
Open Space ⁵	303.9	-	-	-	-	
ROW ⁶	50.4	-	-	-	-	
TOTAL⁷	826.7	-	-	1,439	280,500	

1. Within Jonatkim, developed portion of these areas includes a total 71.9 acres of revegetated open space.
2. School site; but if school district does not purchase, then may be built under the existing RS designation (0.334 du/ac) with up to 726 housing units transferred from other areas, provided Jonatkim properties so not exceed 825 dwelling units total, and City finds housing compatible with adjacent uses
3. Includes up to 2.6 acres Neighborhood Commercial, with the remainder designated as RSF-6.
4. Can be developed as housing or commercial at Planning Commission discretion; housing would come from area 16 total
5. Includes all designated Open Space. Up to 10 acres of this total in subarea 4 may be developed with a recreational facility (with the remainder in private open space), and 2 acres within subarea 5 could be developed with a fire station. Total does not include revegetated and graded open space within developed areas, which totals 71.9 acres.
6. Includes only major rights-of-way for which associated property owner has development responsibility. Local roads not included in total.
7. Includes 0.8 acres (the northern half) of an abandoned Linne Road, which would be transferred to subarea 17 for development.
8. Jonatkim total may not exceed 825 dwelling units among all areas, even though figures add to 857. Dwelling unit potential may be transferred from one area to another, not to exceed the total shown for any given area, subject to Planning Commission approval. The Planning Commission consideration of a request for density transfer within the overall density cap would include the Commission's code defined set of discretionary parameters including but not limited to infrastructure capacity, grading, aesthetics and land use compatibility.
9. Note that for areas 16 and 17, the land use designation suggests allowed densities that do not match the match the maximum allowed dwelling units, based on the acreage of the area shown. This is because the calculation of maximum dwelling units accounts for gross acreage, which for these areas include arterial street ROW. In the event of discrepancies, maximum dwelling counts for these areas shown are considered accurate.



Table 2-2b. Summary of Land Use Acreage, by Property Owner

	Property Owner								
	Jonatkim	Wilcox	CS&G	Rupert ¹	Our Town	Centex	Branch	Meixner	TOTAL
Residential									
<i>Single-Family</i>									
RSF-1	64.0	-	-	-	-	-	-	-	64.0
RSF-2	29.5	-	-	-	-	-	-	-	29.5
RSF-3	69.1	-	-	-	-	-	-	-	69.1
RSF-4	96.8	7.7	20.6	-	-	-	-	-	125.1
RSF-6	34.0	-	53.2	-	-	9.0	-	-	96.2
<i>Subtotal</i>	<i>293.4</i>	<i>7.7</i>	<i>73.8</i>	<i>-</i>	<i>-</i>	<i>9.0</i>	<i>-</i>	<i>-</i>	<i>383.9</i>
<i>Multi-Family</i>									
RMF-8	33.7	-	-	-	-	-	-	-	-
RMF-9	-	-	-	-	12.3	-	-	-	-
<i>Subtotal</i>	<i>33.7</i>	<i>0.0</i>	<i>0.0</i>	<i>0.0</i>	<i>12.3</i>	<i>0.0</i>	<i>0.0</i>	<i>0.0</i>	<i>46.0</i>
Res. Total	327.1	7.7	73.8	0.0	12.3	9.0	0.0	0.0	429.9
Commercial									
NC	-	-	2.6	0.7	-	-	-	-	3.3
CS	-	-	-	-	-	-	11.0	10.0	21.0
Comm. Total	-	-	2.6	0.7	-	-	11.0	10.0	24.3
Public Facility									
PF (school)	18.2	-	-	-	-	-	-	-	18.2
Open Space									
POS	296.8	-	-	-	-	-	4.0	3.1	303.9
Major ROW	30.3	-	8.7	0.6	1.8	4.2	0.3	4.5	50.4
TOTAL	672.4	7.7	85.1	1.3	14.1	13.2	15.3	17.6	826.7

NOTE: Property ownership based on subareas shown in Table 3-1a.

¹ Rupert 0.7 acres shown in NC could be RMF-9 at Planning Commission discretion.

Table 2-2c. Potential Development Summary, by Property Owner

	Property Owner								
	Jonatkim	Wilcox	CS&G	Rupert ¹	Our Town	Centex	Branch	Meixner	TOTAL
Residential (dwelling units)									
<i>Single-Family</i>									
RSF-1	50	-	-	-	-	-	-	-	50
RSF-2	50	-	-	-	-	-	-	-	50
RSF-3	137	-	-	-	-	-	-	-	137
RSF-4	236	31	82	-	-	-	-	-	349
RSF-6	162	-	272	-	-	90	-	-	524
<i>Subtotal</i>	<i>635</i>	<i>31</i>	<i>354</i>	<i>-</i>	<i>-</i>	<i>90</i>	<i>-</i>	<i>-</i>	<i>1,110</i>
<i>Multi-Family</i>									
RMF-8	222	-	-	-	-	-	-	-	222
RMF-9	-	-	-	-	139	-	-	-	139
<i>Subtotal</i>	<i>222</i>	<i>-</i>	<i>-</i>	<i>-</i>	<i>139</i>	<i>-</i>	<i>-</i>	<i>-</i>	<i>361</i>
TOTAL	825 ²	31	354	-	139	90	-	-	1,439
Commercial (square footage)									
NC	-	-	19,000	14,000	-	-	-	-	33,000
CS	-	-	-	-	-	-	150,000	97,500	247,500
TOTAL	-	-	19,000	14,000	-	-	150,000	97,500	280,500

NOTE: Property ownership based on subareas shown in Table 3-1a.

¹ Rupert 0.7 acres shown in NC could be RMF-9 at Planning Commission discretion. Dwellings would be built at the expense of "Our Town" total in subarea 16, so that the two areas do not exceed 139 units.

² Jonatkim total may not exceed 825 dwelling units among all areas, even though figures add to 857. Dwelling unit potential may be transferred from one area to another, not to exceed the total shown for any given area, subject to Planning Commission approval.

³ Subarea 4 could support up to 40,000 SF of private recreational development not reflected in the totals shown in the table.



Grading Approach:

11. The proposed Specific Plan would employ a variety of grading standards, described in Policy LU-13 of Section 3, Plan Goals and Policies, in the Specific Plan. These standards are intended to address the varied topographic nature of the site, and include a combination of mass grading techniques and custom lot grading, depending on the development area. As allowed under Specific Plan law, these standards are tailored to the area, and do not necessarily implement the provisions of the City's existing Hillside Grading provisions.

2.4.4 Specific Plan Policy Framework

The Specific Plan is based on policy direction contained in the City's General Plan, and must be consistent with the General Plan. The following describes the applicable General Plan policies, and the proposed policies that are contained in the Specific Plan.

General Plan Policy LU-2G calls for a Specific Plan to be prepared for the Chandler Ranch Area. The same General Plan policy call for a reduction of 416 dwelling units from a combination of the Chandler Ranch Area Specific Plan and the Olsen-Beechwood Specific Plan Area to ensure that the projected City buildout does not exceed 44,000 in the year 2025.

It should be noted, however, that a 2005 inventory of housing potential throughout the City demonstrated that the reduction of 416 dwelling units from the total potential buildout of the specific plans Candler Ranch and Olsen/Beechwood Specific Plans would not be needed to limit the City's buildout population to 44,000. Instead, each of the specific plans may now develop to their maximum potential without exceeding the population cap of 44,000 (City of Paso Robles, 2005).

Within the scope of a specific plan, the Planning Commission and City Council have the authority to:

- Provide flexibility in terms of:
 - Distribution of densities within the geographic area covered;
 - Parcel sizes and location (including clustering to retain unique site features);
 - Development Standards and other Zoning Ordinance requirements;
 - Allowable land uses by providing an opportunity for mixed use provisions (e.g. neighborhood serving commercial land uses) within the overall residential densities anticipated in the General Plan. This flexibility includes the ability to provide for multi-family land uses as long as the total dwelling unit count is within the scope of the General Plan designation for the geographic area under consideration

- Address community-wide issues on a comprehensive basis, including:
 - Fiscal impacts
 - Infrastructure phasing and financing
 - Parks and Trails



- Project Amenities
- Coordinated Architecture

Within specific plan areas, a fee schedule may be established to provide adequate funding for on- and off-site public facilities and improvements of benefit to properties within the designated specific plan areas. Such fees are above and beyond any property-specific or City-wide property taxes, fees, charges, or assessments.

2.4.5 Specific Plan Goals and Policies

The following goals provide the general framework guiding potential development under the Specific Plan.

- Goal 1** Establish land use and development patterns for the Chandler Ranch Area Specific Plan that respect the underlying topography, protect natural resources, and are compatible with surrounding land uses.
- Goal 2** Development shall balance the interests of private property ownership and the general benefit and welfare of the community at-large with a variety of housing, commercial and open space opportunities.
- Goal 3** Ensure that new development is responsible for the cost of providing City services and infrastructure needs so that the development is fiscally neutral and would not result in a net financial loss for the City.

The following policies are applicable to the Specific Plan area in its entirety:

- Policy 1 Policy and Standard Consistency.** Design and development standards as set out in this plan and in other City plans, policies and ordinances adopted and in effect at the time of any design review shall be applied to all projects in the Plan Area to ensure the highest possible quality and character of development. Where the standards and conditions prescribed by this plan are more restrictive than in other City policy documents, those within this plan shall prevail.
- Policy 2 Comprehensive Design.** Owners of contiguous parcels shall be encouraged to collaborate on development proposals to better integrate future development within the entire Specific Plan area.
- Policy 3 Phased Development.** For any development to be implemented in more than a single phase, the developer(s) shall be required to provide a detailed phasing plan to the City. This plan will be applicable to all future development of the subject property and shall be enforceable upon all future owners and/or developers of the property.
- Policy 4 Improvement Costs.** All costs for public improvements and facilities required to support any development of the Plan Area, including the fair share of costs for offsite improvements and facilities which may be required to mitigate the impacts of Chandler Ranch development on other portions of the City, shall be borne fully by



such development. The City may collect or recover such costs through appropriate and effective funding mechanisms identified in this plan. For those improvements and facilities that are within the plan area that are necessary for the buildout of the plan area but may be of regional benefit (e.g. Huerhuero bridge; Airport Road connection to Highway 46 East), the City Council will consider requests for reimbursement agreements where City studies document that it would be reasonable and appropriate.

The Specific Plan also includes policies related to the following topics:

- Land Use (including grading)
- Circulation
- Infrastructure

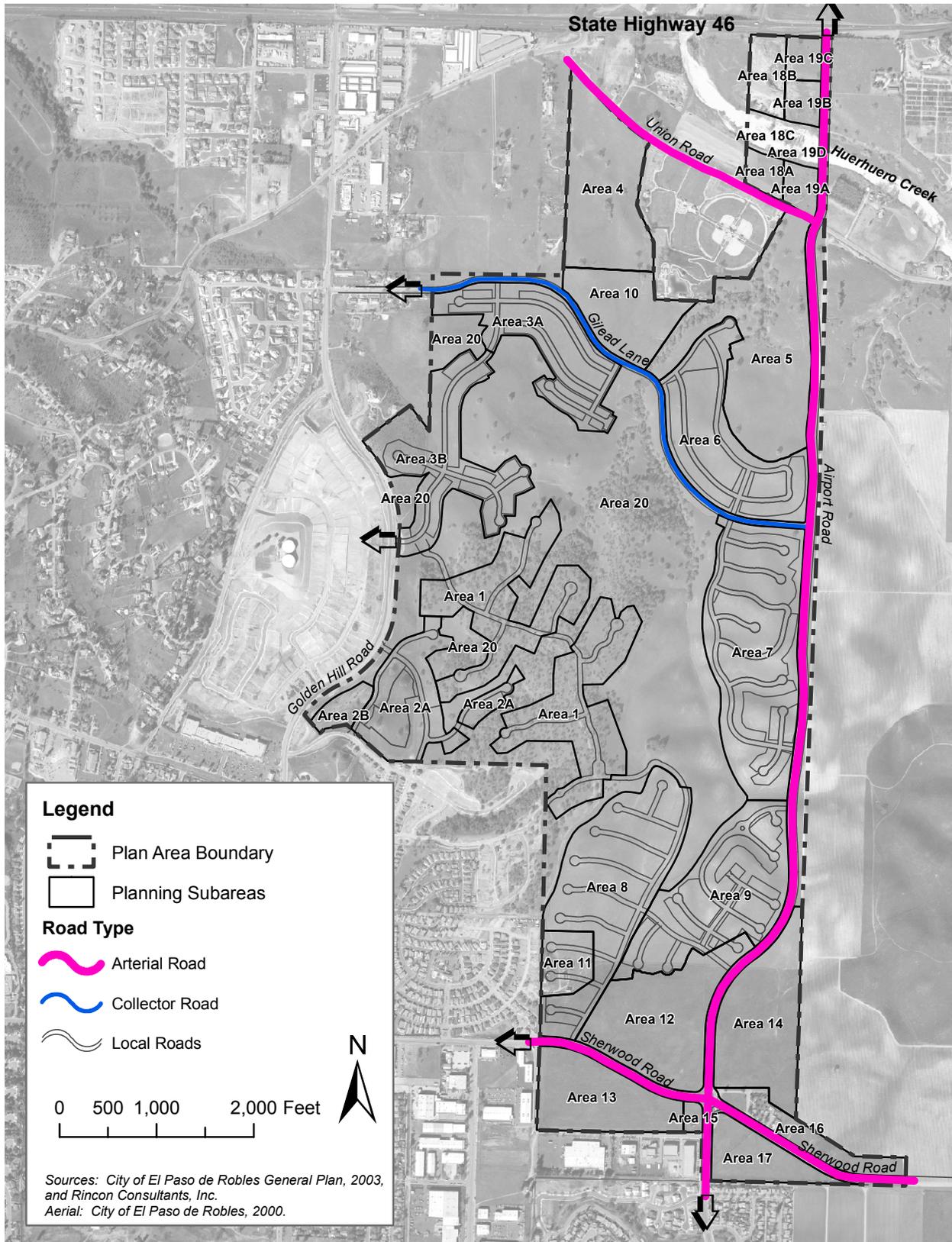
Additional policies are included to address development within each subarea. These policies discuss the development parameters of each area, including maximum number of dwelling units and commercial potential, grading requirements, and the infrastructure required prior to the development of a particular area. Development standards specific to each subarea are also included. These typically relate to natural resource protection, visual impacts, agricultural buffers, and noise attenuation techniques.

2.4.6 Circulation and Public Utility Infrastructure

a. Circulation System. The Chandler Ranch Area Specific Plan delineates the major roadways needed to implement the City's General Plan Circulation Element. Generally speaking, the Specific Plan is a crucial link to complete part of the City's primary north-south and east-west circulation system. Specifically, the circulation plan would link interior roadways with existing arterials, including Union Road, Gilead Lane, Golden Hill Road, Fontana Road and Linne Road. Airport Road would follow the eastern boundary of the Specific Plan area, providing a physical buffer from adjacent agricultural lands. Linne Road would be realigned to more directly connect with Sherwood Road. Figure 2-3 illustrates the Specific Plan circulation system, while Tables 2-3 and 2-4 show the primary onsite improvements and standards needed to implement the plan. Table 2-3 also shows major offsite improvements needed at various thresholds associated with development under the Specific Plan.

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Circulation Plan

Figure 2-3

Table 2-3. Specific Plan Circulation Improvements

On-Site (to be constructed by developer, based on Subarea Standards shown in Section 3.5 of the Specific Plan)
<i>Roadway extensions or realignments</i>
<ul style="list-style-type: none"> • Airport Road extension between Highway 46 East and Union Road to arterial standards • Airport Road extension between Union Road and Linne Road to arterial standards • Extension of Sherwood Road to arterial standards • Extension of Gilead Lane to Airport Road to collector standards • Golden Hill and Union Road frontages to be constructed to 4-lane arterial standards
<i>Intersections/Traffic Control</i>
<ul style="list-style-type: none"> • Wherever feasible, traffic calming measures shall be designed and implemented instead of installing traffic signals and/or traffic control devices that tend to hinder constant traffic flow. When traffic calming measures are not feasible, traffic signals and/or other controls shall be installed at locations designed by the City Engineer.
<i>New Bridges/Crossings</i>
<ul style="list-style-type: none"> • Airport Road bridge/crossing over unnamed drainage in east- central portion of site • Gilead Lane bridge/crossing over central drainage feature
<i>Pedestrian/Bikeway Facilities</i>
<ul style="list-style-type: none"> • Chandler Ranch Area Specific Plan trail system, which may include all-weather creek crossings • Traffic Calming Measures where appropriate/applicable • LED lighted crosswalks, especially near school
On-Site (costs to be shared by Specific Plan property owners)
<i>Roadway extensions and bridges</i>
<ul style="list-style-type: none"> • Airport Road bridge over Huerhuero Creek • Applicable share of Airport Road connection to Highway 46 East
Off-Site (to be constructed by City through fees paid in accordance with Section 4.0 of the Specific Plan; timing thresholds are shown below)
<i>Based on existing traffic conditions, the following locations will require improvements with initial development of the CRASP as current Levels of Service are at or below acceptable levels:</i>
<ul style="list-style-type: none"> • <u>State Route 46 East/US 101 intersection.</u> An interim improvement is being proposed by Caltrans to add dual left turn lanes in the westbound direction of SR 46E for the southbound U.S. 101 on-ramp. With this improvement, an additional westbound through lane will be added at the northbound ramp intersection, which will add sufficient capacity to improve Level of Service at this intersection. • <u>Union Road/SR 46 East intersection.</u> Subject to Caltrans approval, modify intersection to right turn only from Union Road to SR 46 East, prohibiting a left turn from Union Road to SR 46. With prohibition of left turn movement, this Level of Service and safety problem could be mitigated to acceptable levels. This improvement would also relieve future projected Level of Service problems at the Union Road/Union Road extension intersection. • <u>Union Road/Golden Hill Road intersection.</u> Subject to Caltrans approval, signalization and some additional widening or creation of a roundabout would provide interim traffic congestion relief and improve Levels of Service to acceptable conditions.
<i>Improvements needed before the 500th dwelling unit is constructed for subareas south of Gilead Lane:</i>
<ul style="list-style-type: none"> • <u>Golden Hill Road/SR 46 East.</u> Intersection to be improved in accordance with EIR mitigation. • <u>Rolling Hills Road/Golden Hill Road.</u> Intersection to be improved in accordance with EIR mitigation. • <u>Niblick Road/Creston Road.</u> Intersection to be improved in accordance with EIR mitigation.
<i>Improvements needed before full commercial development north of Gilead Lane:</i>
<ul style="list-style-type: none"> • <u>Golden Hill Road/SR 46 intersection.</u> Recognizing that commercial development north of Gilead Lane is limited, particularly if access to SR 46E is restricted to a right turn movement only at the Union Road/SR 46E intersection, further improvements should occur at the Golden Hill Road/SR 46E intersection before full development of the commercial uses north of Gilead Lane. • <u>Airport Road/SR 46 East intersection.</u> A Project Study Report (PSR) is being prepared that is intended to evaluate both interim and long-term improvement needs for this intersection. Subject to Caltrans approval, interim improvements may consist of an at-grade signalized intersection. The PSR will determine long-term improvement needs including right-of-way requirements for the long-term needs. A connection from Airport Road to SR 46E should be made prior to full development of commercial uses north of Gilead Lane.
<i>Improvements needed before the 1,200th dwelling unit is constructed within the CRASP; or before the 1,050th dwelling unit is constructed in combination with the full development of the CRASP commercial area fronting Niblick/Sherwood Road and the full development of the CRASP commercial area bounded by Union Road and Huerhuero Creek:</i>
<ul style="list-style-type: none"> • <u>Golden Hill Road/SR 46 East.</u> Assuming that SR 46E remains a four-lane divided arterial through its intersections with Golden Hill Road and that residential development occurs before all commercial development, 1,200 residential units (80% of total) within the CRASP could be accommodated with the following intersection configuration at SR 46E/Golden Hill Road: <ul style="list-style-type: none"> ○ Northbound and southbound Golden Hill Road – one lane for each turn movement (left, through, right) ○ Eastbound and westbound SR 46E – one left-turn lane, two through-lanes, one right-turn lane. ○ Signalize all approaches with protected phasing.

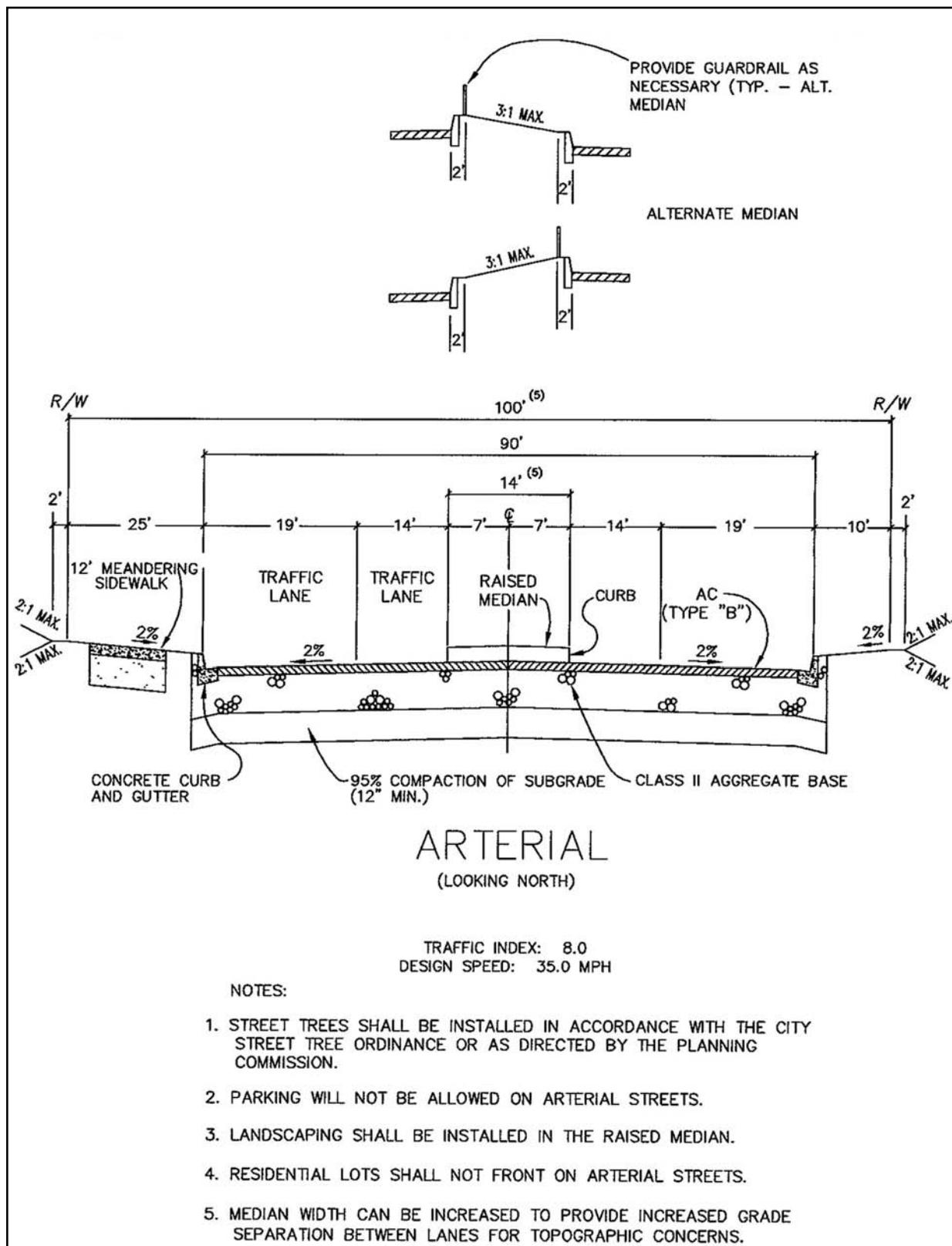


Table 2-4. Specific Plan Roadway Standards

Roadway Classification	Facility	General Specifications	Example Cross-Section
Arterial	Airport Road	<ul style="list-style-type: none"> Extended from Highway 46 East at the northern boundary of the Plan Area to Linne Road at the southern end. Anticipate need for right-of-way for future interchange at Highway 46 East Designed to minimize required grading and will include maximum traffic calming measures to ensure that vehicles maintain a safe speed. The road shall be divided and include a variable median size to allow for grade separation between the north and south lanes to minimize grading. The roadway will include four through lanes, a bike lane, a landscaped parkway, and a pedestrian/ bicycle pathway (which may be separated). Parking will not generally be permitted along Airport Road and sidewalk/pathway is only necessary on the west side. 	Figure 2-4 (Figure 3-11 of Specific Plan)
Arterial	Sherwood Road	<ul style="list-style-type: none"> Sherwood Road would be extended from its current eastern end to the east side of the Specific Plan Area. Linne Road would no longer be the primary through roadway serving the southern portion of the Plan Area. Sherwood Road will include four through traffic lanes, a landscaped median, a bike lane, sidewalk, a landscaped parkway and a pedestrian/bicycle path. The intersection with Airport Road to be designed as a roundabout; if not feasible in the opinion of the City Engineer, intersection should be signalized. 	City Standard
Arterial	Union Road	<ul style="list-style-type: none"> Will be improved to four-lane arterial status per City standard; if feasible, will include installation of a roundabout at the intersection with Airport Road. 	City Standard
Arterial	Golden Hill Road	<ul style="list-style-type: none"> Will be improved to four-lane arterial status. Golden Hill Road at the intersection with SR 46E to be designed with one lane for each turn movement (left, through, right). Signalize all approaches at the intersection with SR 46E. 	City Standard
Collector	Gilead Lane	<ul style="list-style-type: none"> Gilead Lane would be extended from its current eastern end, through the Plan Area to intersect with Airport Road extension. There will be no driveways along Gilead Lane and parking will be limited to parking pockets among landscaped bulbouts. The intersection with Airport Road to be designed as a roundabout; if not feasible in the opinion of the City Engineer, intersection should be signalized. Gilead Lane will include two travel lanes, two bike lanes, and sidewalks on both sides. A landscaped median may be an option in places 	Figure 2-5 (Figure 3-12 of Specific Plan)
Local	All internal residential roadways	<ul style="list-style-type: none"> Internal residential roadways will be improved to local road specifications, consistent with City standards, unless otherwise specified within the standards for a particular subarea. Local roads include two traffic lanes, parking on one side and a standard sidewalk on at least one side. 	City Standard

Note: All arterials shown are based on a 100-foot right-of-way. All streets shall incorporate traffic calming design to the extent feasible.

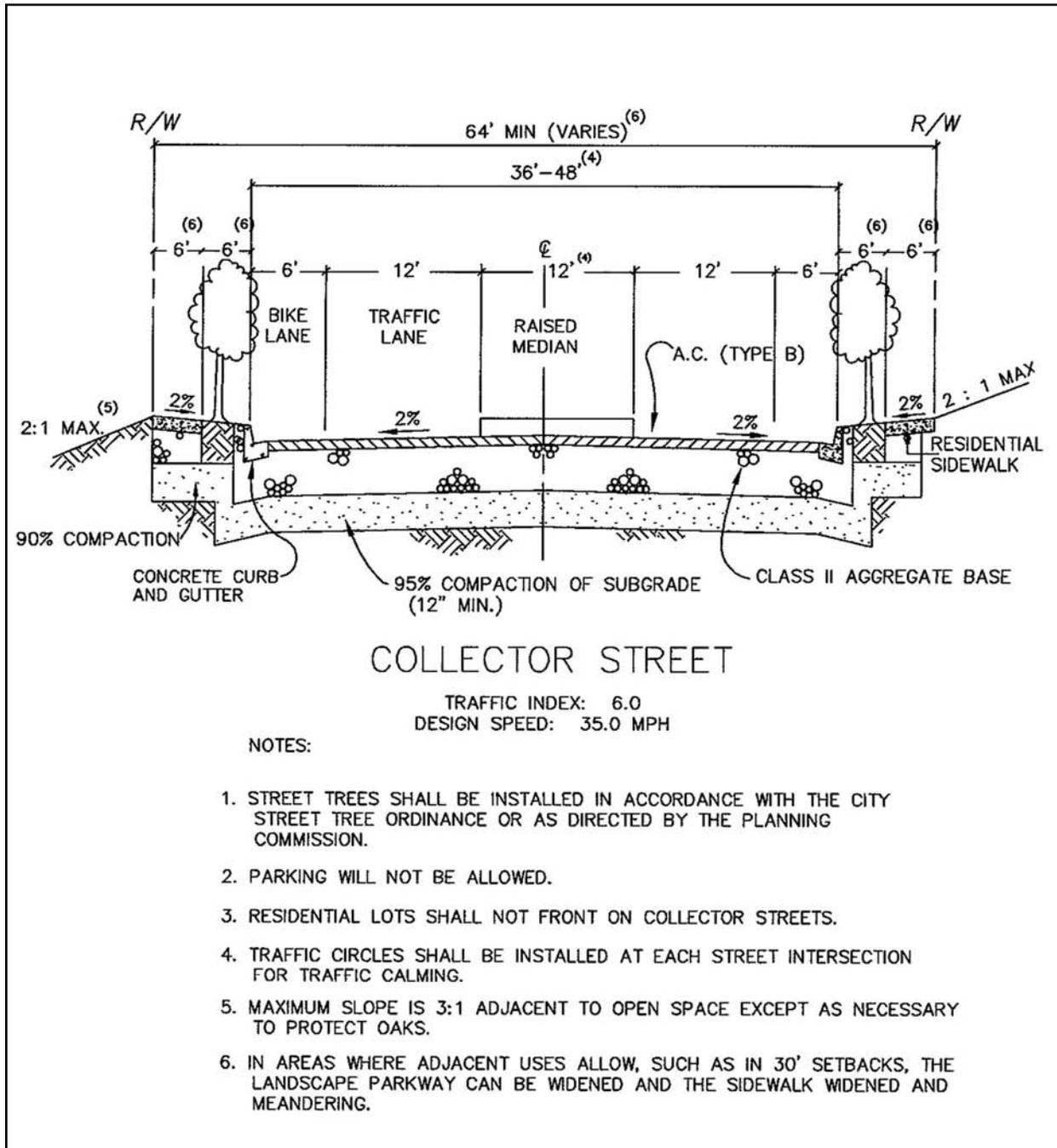




Source: City of Paso Robles, November 2005.

Airport Road Cross Section

Figure 2-4



Source: City of Paso Robles, November 2005.

Gilead Lane Cross Section

Figure 2-5

The discussion below clarifies the thresholds used to determine the timing of the improvements shown in Table 2-3.

Concurrent with Initial Development of CRASP Area.

Based on existing traffic conditions, the following locations will require improvements with initial development of the CRASP project area as current Levels of Service are at or below acceptable levels:

- State Route 46 East/US 101 NB Ramps
- State Route 46 East/US 101 SB Ramps
- Union Road/SR 46 East
- Union Road/Golden Hill Road

For SR 46E/US 101 SB Ramp intersection, an interim improvement is being proposed by the City of Paso Robles and Caltrans to add dual left turn lanes in the westbound direction. With this improvement, an additional westbound through lane will be added at the SR 46E/US 101 NB Ramp intersection, which will add sufficient capacity to improve Level of Service at both of these intersections. Due to funding limitations on constructing the improvements, it is not anticipated to be operational until 2015. As CRASP develops, the project will contribute its fair share to the mitigation. However, the near term LOS will degrade until the improvement is complete and operational.

Level of Service and safety problems are projected for mitigation to acceptable levels at the Union Road/SR 46E intersection with the prohibition of left turn movements from the northbound approach. This improvement would also relieve future projected Level of Service problems at the Union Road/Union Road Extension intersection.

At the Union Road/Golden Hill Road intersection, signalization and some additional widening or construction of a roundabout would provide interim traffic congestion relief and improve Levels of Service to acceptable conditions.

Area South of Gilead Lane.

Although the development timing of both residential and commercial development south of Gilead is not known, a threshold of no more than 500 single and multi-family dwelling units should be developed without improvement, consistent with the identified mitigation measures, to the following intersections.

- Golden Hill Road/SR 46E
- Rolling Hills Road/Golden Hill Road
- Niblick Road/Creston Road

With improvement to these intersections, the balance of the residential uses along with the commercial uses could be constructed.

Area North of Gilead Lane.

Commercial development of the CRASP area north of Gilead Lane is limited, particularly if access to SR 46E is restricted to a right turn movement only at the Union Road/SR 46E intersection (i.e. no northbound left-turn movements are permitted). Recognizing the limited access at the site from the highway, further improvements, as follows in the subsequent paragraph, should occur at the Golden Hill Road/SR 46E intersection and at the Airport



Road/SR 46E connection before full development of the commercial uses is allowed north of Gilead Lane.

Golden Hill Road is currently operating at LOS “D” during the weekday PM peak hour period, on the cusp of LOS “C/D” (35.0s). With the completion of ongoing construction at the intersection (e.g. gas stations), the intersection is projected to operate at LOS “E”. Assuming that SR 46E remains a four-lane divided arterial through its intersections with Golden Hill Road and that residential development occurs before all commercial development, the following intersection configuration at SR 46E/Golden Hill Road would allow for 1,200 residential units (80% of total) within CRASP to be accommodated at acceptable LOS:

- Northbound and southbound Golden Hill Road - one lane for each turn movement (left, through, right)
- Eastbound and westbound SR 46E - one left-turn lane, two through-lanes, one right-turn lane.
- Signalize all approaches with protected phasing.

With the same intersection configuration, 1,050 residential units (72% of total) within CRASP could be accommodated concurrently with the full development of the CRASP commercial area fronting Niblick/Sherwood Road (equivalent to 150 PM peak hour trips) and development equivalent to 200 PM peak hour trips on the CRASP commercial area bounded by Union Road and Huerhuero Creek. As previously stated, the development of the CRASP commercial area bounded by SR 46E and Huerhuero Creek would not be possible without an Airport Road/SR 46E connection.

b. Public Utility Infrastructure. The Specific Plan describes how future development would be provided public utilities, water, wastewater, and stormwater facilities, as well as natural gas, electrical, telephone, and cable service. These utilities are each discussed briefly below and in more detail in the Specific Plan.

Water Supply. Currently, City wells furnish nearly all of the water supply for urban use, and a limited number of private wells serve agricultural uses within the city limits. Sixty-two County area parcels are served with City water. According to the General Plan, the existing water supply system has the potential to provide approximately 14,000 AFY based on current permitting and water rights, however the current system can only produce approximately 8,000 AFY. Existing wells drawing from shallow alluvial river underflow are currently drawing at the maximum permitted amount of 4,600 acre feet per year or 8 cfs. Additional supplies through riparian rights for water from the deeper, Paso Robles Greater Groundwater basin are available to support additional production.

The following policy provisions are included in the Specific Plan to address water supply to new development within the Specific Plan area:

Policy I-6 Water System. All new development in the Chandler Ranch Area Specific Plan shall be required to connect to City water services in accordance with the City Municipal Code and the Water Service Master Plan. The system shall also be consistent with the preliminary Water System Plan shown in Figure 2-6.



Policy I-7 **Looped System.** The water system shall be a looped system consistent with the layout shown in Figure 2-6. All development shall be connected to a looped system prior to occupancy.

Policy I-8 **New Water Supply Wells.** Prior to development, an increase in production of the City's water supply will be required. It is anticipated that for total buildout of the Chandler Ranch Specific Plan area, a minimum increase in production of 1,630 GPM will be required (refer to Boyle Engineering, March 24, 2005 memo to Bob Lata, City of Paso Robles). This production could be provided by three new wells, each with a production capacity of approximately 650 GPM (or equivalent capacity) installed within the Specific Plan area to augment the City's current water demand and delivery service capacity. Wells shall be developed, and/or fair share contributions made as necessary to supply water to each phase or area of development. These well sites must be easily accessible with the development area reserved for each approximately 100'x100' in size. All new wells must be deep wells drawing from the Paso Robles Greater Groundwater Basin. There is currently no additional permitted capacity available from the shallower alluvial river water well water sources.

Policy I-9 **Water Storage Responsibility.** New development within the Chandler Ranch Area Specific Plan would be responsible for 1.70 MG of water storage (refer to Boyle Engineering, March 24, 2005 memo to Bob Lata, City of Paso Robles). Specific Plan development must pay its fair share toward the construction of offsite storage facilities.

As a follow-up to the General Plan update and in order to ensure that adequate facilities will be available to serve the Chandler Ranch Area Specific Plan, the City's water storage capacity is currently under study. The water system analysis includes an update of the City's water system master plan and alternative sites for water reservoirs. For the purpose of the Draft Chandler Ranch Area Specific Plan and Draft Environmental Impact Report, at this point in time it is not possible to rule out the potential need to locate a water reservoir site within the Chandler Ranch Area Specific Plan. The water system exhibit for this specific plan identifies three potential locations for two 6-million gallon tanks to supplement the city's water storage supply. A determination to place reservoirs on the Chandler Ranch Specific Plan area may be made by the time the Final Specific Plan and Final EIR are ready for consideration. It should be noted that if the water tank capacities are ultimately found to exceed the needs of the Specific Plan area, a separate CEQA evaluation of these tanks will be required outside the context of this EIR, since this project is not the responsibility of the Chandler Ranch Area Specific Plan, but is instead an effort to address Citywide water needs.

Policy I-10 **Water Distribution.** Prior to development, in the Orchard Bungalow Water Zone, a water distribution system analysis will need to be performed to determine any excess capacity of the existing Orchard Bungalow Booster Pump Station that may be available. Additional boosting capacity shall be provided and/or fair share contributions made as necessary to distribute water to each



phase or area of development. If additional boosting capacity is provided, it may be accomplished by new or modified booster systems as approved by the City. Appropriate PRV valves shall be situated to separate the two zones on the site. The distribution system shall be modeled and project specific fire flow analysis to validate main sizes shown in the Water Master Plan, and determine other system improvements required to meet the City's demand and fire flow criteria.

Policy I-11 Fire Flow Requirements. The water system shall be designed to meet the City's demand and fire flow criteria.

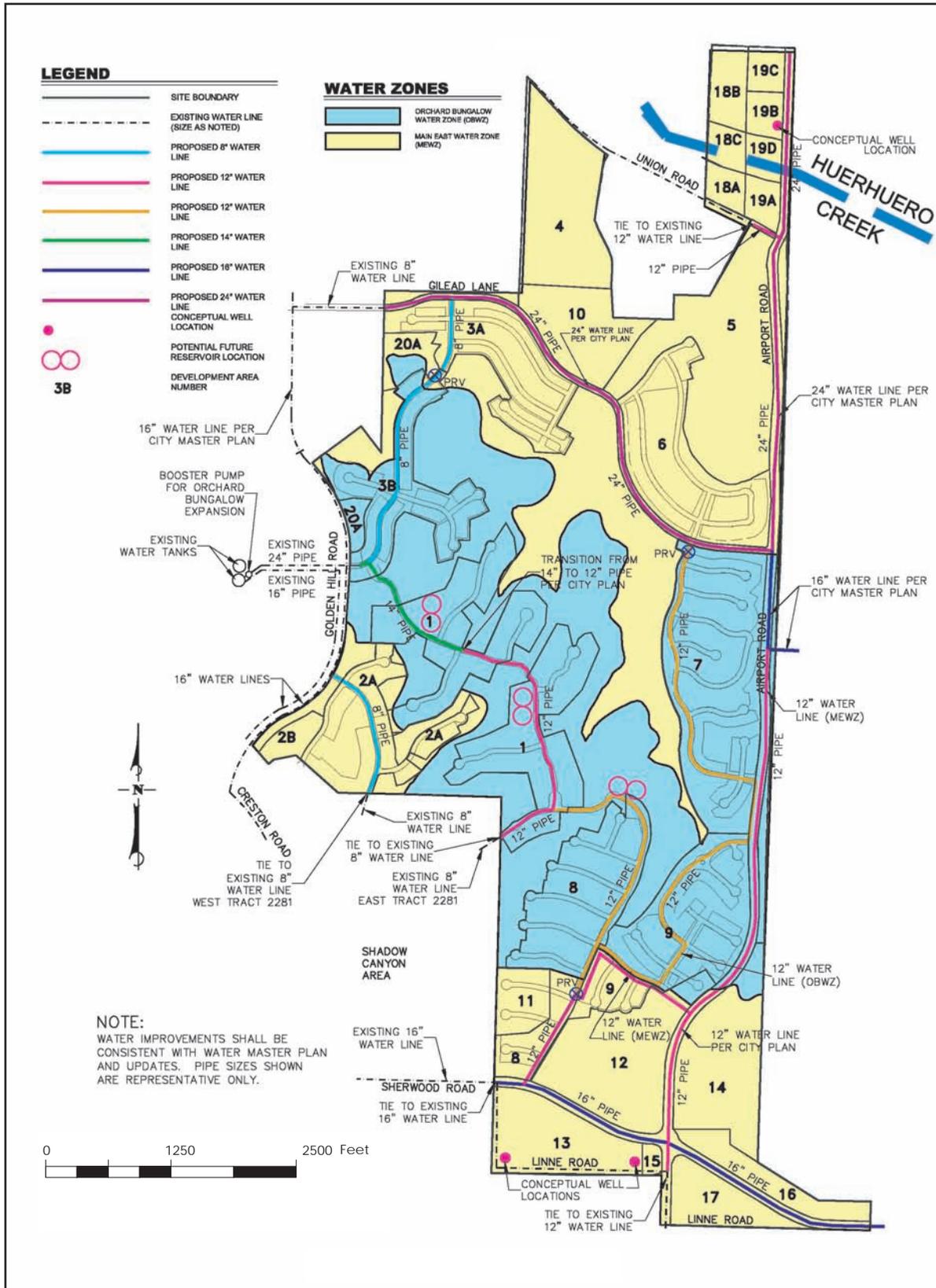
Policy I-12 Extension of Utilities. Each development shall extend and stub out all utilities to at least the project/property boundary to facilitate orderly development.

Wastewater. All new development in the Plan Area will be required to connect to City sewer service in accordance with the provisions of the City Municipal Code and consistent with the Sewer Service Plan in Figure 2-7. The Specific Plan includes the following policy direction with respect to the provision of wastewater infrastructure within the Specific Plan:

Policy I-13 Sewer System. All new development in the Plan Area shall be required to connect to City sewer service in accordance with the provisions of the City Municipal Code and consistent with the Sewer System Plan in Figure 2-7.

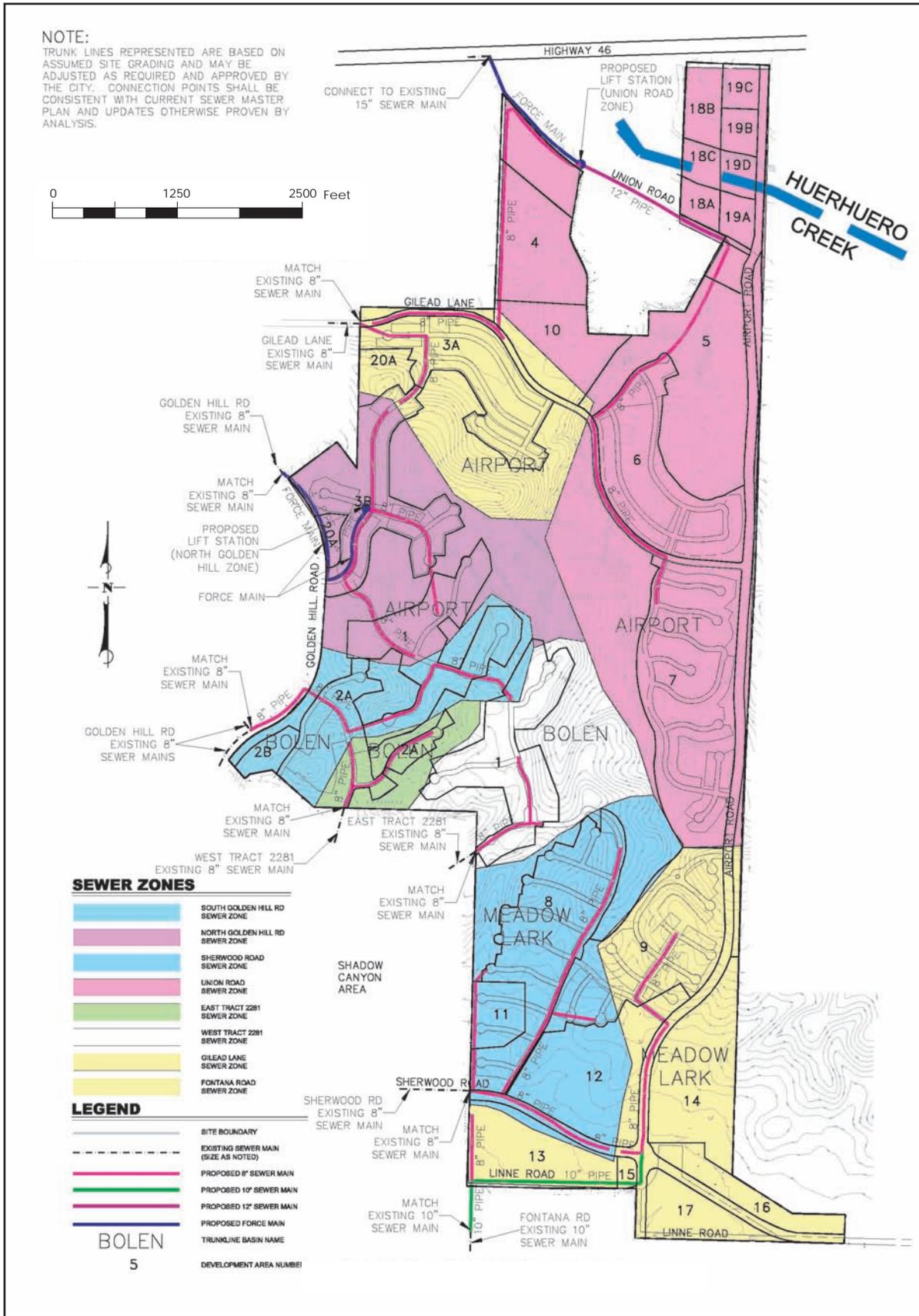
Policy I-14 Sewer Treatment Capacity. Prior to any development application being approved within the Chandler Ranch Specific Plan Area, sewer treatment plant capacity must be verified within the context of additional development entitlements beyond the last Sewer Master Plan in a manner consistent with City requirements, and the development would be required to pay any project specific mitigation fees or meet other applicable requirements.

Policy I-15 Sewer Trunkline Capacity. Existing sewer collection facilities downstream of the Chandler Ranch Specific Plan area are currently unable to accommodate the additional flows from new development. This has been demonstrated in the 1993 Sewer Master Plan Update and the April 20, 1999 Potential Deficiencies Report from the Wastewater Division. Continuing deficiencies have been confirmed in a limited scope analysis performed in the preparation of this Specific Plan. Impacts may not be limited to the Meadowlark Sewer Basin only, although an emphasis on previous analysis has been on the Meadowlark Sewer Basin related sewer collection system. An effort is currently underway by Boyle Engineering under contract with the Public Works Department of the City of Paso Robles to model and identify a comprehensive list of system deficiencies. Prior to any development that will discharge into any downstream sewer basin, improvements need to be constructed and/or fair share contributions toward the



Source: Penfield & Smith, March 2006.

Chandler Ranch Area Specific Plan EIR
 Section 2.0 Project Description



Source: Penfield & Smith, March 2006.

Sewer System Plan

Figure 2-7

cost of improvements made in accordance with the wastewater Master Plan Update (being Prepared by Boyle). See also the subarea specific requirements in Section 3.5 of the Specific Plan for subareas 8, 9, and 11-17.

Drainage. All new development in the Plan Area shall be required to provide drainage facilities in accordance with the provisions of the City Municipal Code and consistent with the Drainage Plan in Figure 2-8. The following policy provisions are included in the Specific Plan to address drainage infrastructure to accommodate new development within the Specific Plan:

Policy I-16 Drainage Facilities. All new development shall be required to provide drainage facilities in accordance with the provisions of the City Municipal Code and consistent with the Drainage Plan in Figure 2-8 and detention basin summary in Table 2-5. Drainage easements shall be established as necessary.

Table 2-5. Detention Basin Summary

Detention Basin No.	Contributory Drainage Area Developed/(Total Acres)	Maximum Allowable Discharge ($Q_{10\text{ predev}}$) (cfs)	Calculated Basin Storage (Acre-ft)
2	9.3 (14.4)	7.2	0.17
3B	20.0 (28.3)	19.4	0.31
4	7.4 (9.9)	7.1	0.06
5	33.3 (51.3)	29.0	0.51
6	9.7 (12.8)	9.5	0.26
7	48.6 (65.4)	48.8	0.56
8A	45.1 (83.6)	45.0	1.04
8B	170.9 (379.8)	161.4	5.30
8C	16.3 (34.2)	14.6	0.89
10	25.4 (34.2)	25.4	0.24
11	28.0 (37.7)	26.2	0.48
12	11.1 (15.2)	11.1	0.14
13	17.0 (20.4)	16.9	0.16
14A	36.7 (80.4)	34.6	1.53
14B	7.9 (26.3)	7.8	0.67
14C	29.8 (44.1)	26.2	0.76
14D	20.6 (28.7)	20.6	0.21
15A	16.9 (26.6)	16.9	0.19
15B	34.1 (64.5)	34.1	0.50
15C	4.1 (12.4)	4.0	0.29

Table 2-5. Detention Basin Summary

Detention Basin No.	Contributory Drainage Area (Acres)	Maximum Allowable Discharge ($Q_{10\text{ predev}}$) (cfs)	Calculated Basin Storage (Acre-ft)
2	14.4	9.3	0.17
3B	28.3	20.0	0.31
4	9.9	7.4	0.06
5	51.3	33.3	0.51
6	12.8	9.7	0.26
7	65.4	48.6	0.56
8A	83.6	45.1	1.04
8B	379.8	170.9	5.30
8C	34.2	16.3	0.89
10	34.2	25.4	0.24
11	37.7	28.0	0.48



Table 2-5. Detention Basin Summary

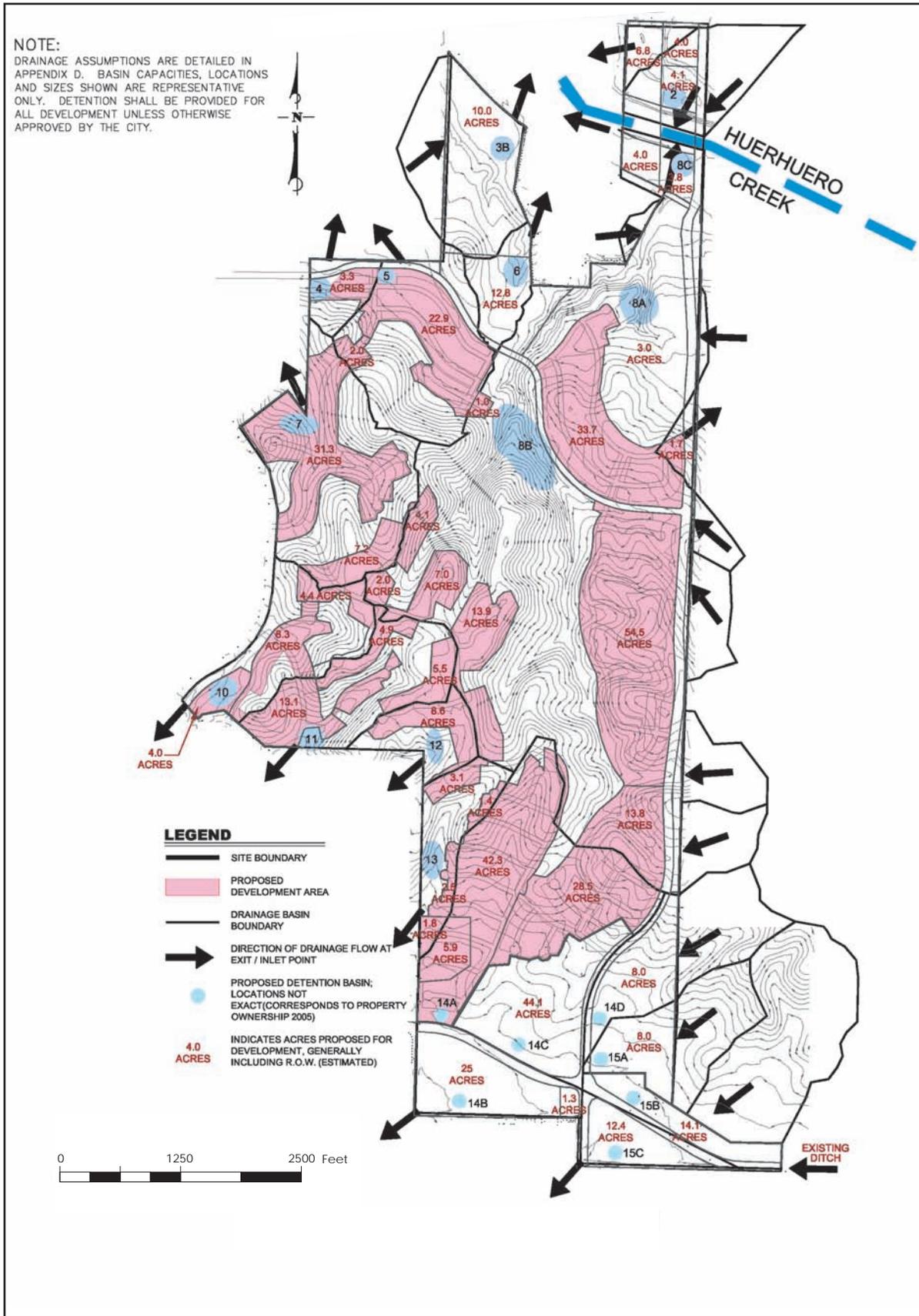
Detention Basin No.	Contributory Drainage Area (Acres)	Maximum Allowable Discharge (Q_{10 predev}) (cfs)	Calculated Basin Storage (Acre-ft)
12	15.2	11.1	0.14
13	20.4	17.0	0.16
14A	80.4	36.7	1.53
14B	26.3	7.9	0.67
14C	44.1	29.8	0.76
14D	28.7	20.6	0.21
15A	26.6	16.9	0.19
15B	64.5	34.1	0.50
15C	12.4	4.1	0.29

Policy I-17 Drainage and Detention Basin Design. Detention to mitigate post-development increases in peak run-off will be required. Figure 2-8 and Table 2-5 show representative detention possibilities and representative detention basin capacities based on preliminary assumptions. Detailed analysis & design shall be required with any proposed development. Detention basin locations may be cooperatively located by adjacent property owners in order to share a single detention basin, or each property may develop its own.

Detention basins shall be designed in accordance with applicable City standards and current City Practice as directed by the City Engineer. The design must consider the volume of water that the detention basin is expected to store as well as operation and maintenance of the detention basins. The detention basins are to have a filtering device on the inflow side to prevent the flow of contaminants and sediments into the detention basins. Detention basins shall be designed to meet the following standards:

- a. Volume: Detention basins shall be sized to provide capacity for a 100-year storm event (minimum) and to meet the outflow requirements listed below.
- b. Outflow Device: All detention basins are to be designed to be free draining. Terminal basins (i.e., pumped basins) are not allowed. Outlet pipes shall be oversized (18-inch minimum) with an orifice restriction to limit outflow to Q₁₀ pre-development flows or as determined by the City. Orifice restriction plates shall be removable for emergency situations. A removable trash rack shall be provided at the outlet. Current City drainage water quality policy requires that detention basins must be designed to capture summertime (dry season) irrigation run-off and other nuisance water and preclude discharge of this water into natural drainage courses.
- c. Slopes: Maximum side slopes shall be four horizontal to one vertical on interior slopes and two horizontal to one vertical on exterior slopes. A soils engineering and geotechnical report shall be required for all fill levee sections. The report shall address remedial grading, benching, and slope stability of the level sections.
- d. Emergency Overflow: An emergency overflow spillway shall be sized for the peak 100-year storm runoff. The spillway shall be engineered and shall be reinforced concrete. The spillway should be designed with a minimum of one foot of freeboard above the 100-year spill water surface elevation.





Source: Penfield & Smith, March 2006.

Drainage Plan

Figure 2-8
 City of El Paso de Robles

- ~~e. Low Flow Drainage: The bottom of the detention basin shall have a minimum gradient of 2% draining to the outlet, or a low flow reinforced concrete swale shall be provided with a minimum gradient of 0.5% draining to the detention basin outlet.~~
- e. Access Ramp: A maintenance access ramp shall be provided down into the detention basin in a manner and dimensions acceptable to City staff.
- f. Locating and Landscaping. The City shall require review and approval of any proposed basin landscape plan. Landscaping shall be selected to minimize maintenance, while minimizing impact to native and sensitive species that could be harmed by invasive plant species. To the extent possible, such facilities shall be placed to avoid removing oak trees, and should not be designed to serve as informal parks. No trees or shrubs shall be planted within 15 feet of the detention basin outlet. Basins should be located to minimize regulatory compliance issues and where soils are suitable for such facilities or soil must be imported to replace the unsuitable soils. Floating objects such as railroad ties and landscape bark are not permissible.
- g. Maintenance: Prior to final development approval, the applicant shall either be deeded to the City in fee, or the developer shall enter into a maintenance agreement with the City to assure perpetual maintenance of the detention basin and related on-site private drainage improvements and to allow the City emergency access. Fences are required if the maximum water depth attainable exceeds 4 feet and the side slopes are steeper than 4 to 1.
- h. Mosquito Abatement: The City shall require review and approval of detention basins for public safety and mosquito abatement.

A developer may request to be able to make up any lost density from using developable land for detention basins through clustering dwelling units. Any applicants under the Specific Plan shall provide an engineered hydrologic analysis and drainage plan for the project, prepared by a qualified engineer, which evaluates the added runoff that would result from site development, in relation to the existing drainage system. The hydrologic analysis shall specify design standards for drainage facilities that would adequately convey storm water runoff under 100-year flood conditions in accordance with City standards. The stormwater conveyance devices shall be sized to accommodate the expected flows, up to a Q25 event with freeboard, and also designed to withstand a Q100 event without damage to any proposed structure.

Any future structures planned within the designated 100-year flood zone shall include plans that indicate the construction of floor levels at a minimum of one foot above base flood elevations.

Policy I-18 Storm Drainage Construction Responsibility. The Specific Plan provides a framework for addressing storm drainage detention requirements. Property owners may reach agreements and propose common facilities to provide equivalent functions. Any changes to the framework presented in the Specific Plan shall be subject to approval of the City Engineer.



2.4.7 Specific Plan Phasing and Implementation

a. Phasing. Development under the specific plan will not be phased in the traditional sense, as the actual rate of development will not be subject to any growth management controls. Rather, it will be based on market demand, and the ability for key infrastructure to be implemented prior to certain phases of development. Thresholds have been established that set the standards for infrastructure and improvements that are necessary prior to each subarea's development. Except when a Specific Plan policy calls for accelerated construction of infrastructure, as development occurs the infrastructure system planned for the Chandler Ranch Area will be constructed as necessary to accommodate the development within a specific subarea, as shown in Tables 2-6 through 2-24. It should be noted that infrastructure requirements shown in these tables should be considered guidelines for development, with site-specific evaluations to be conducted at the time of the entitlement process.

Table 2-6. Subarea 1 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> Golden Hill Road widened to 4 lane arterial standards adjacent to subarea 1 Area 1 local road from Golden Hill Road to Shadow Canyon, providing two points of access All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> Construction of Detention Basins 7, 10, 11, 12, 13 and 8b, or other detention basins designed to mitigate post-development flows as applicable.
Water
<ul style="list-style-type: none"> Construction of (OBWZ) booster pump near Golden Hill Road. 14" water main from (OBWZ) booster pump to 14"-12" transition point on Area 1 internal local road 12" water main (OBWZ) from 12"-14" pipe transition point described above, to existing east Tract 2281 8" water main via new road improvement alignment.
Sewer
<ul style="list-style-type: none"> 8" sewer main within North Golden Hill Road Sewer Zone, and associated 4" force main to connection point to existing 8" sewer main on northern part of Golden Hill contiguous to this zone. 8" sewer main from South Golden Hill Sewer Zone to 8" existing sewer main on south part of Golden Hill Road contiguous to the zone. 8" sewer main from East Tract 2281 Sewer Zone to existing East Tract 2281 8" sewer main.

Table 2-7. Subarea 2 Onsite Infrastructure Requirements

Traffic Circulation
<ul style="list-style-type: none"> Golden Hill Road frontage improvement to 4-lane arterial Area 2 local road from Golden Hill Road to Shadow Canyon All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> Construction of Detention Basins 10 and 11, or other detention basins designed to mitigate post-development flows as applicable.
Water
<ul style="list-style-type: none"> 8" water main from existing Golden Hill Road 24" main to existing west Tract 2281 8" water main via proposed road.
Sewer
<ul style="list-style-type: none"> 8" main from the high point in the Area 2A (north) local road to the existing 8 inch main in Golden Hill Road. 8" main from high point in the Area 2A (south) road to the existing 8" main in Tract 2281 West



Table 2-8. Subarea 3 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Extension of Gilead Lane into Area 3 as a collector internal-local road • Golden Hill Road access to Area 3 to internal local road • Area 3 internal local roads per Specific Plan standards • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins 4, 5, and 7.
Water
<ul style="list-style-type: none"> • Construction of (OBWZ) booster pump near Golden Hill Road • 14" water main from (OBWZ) booster pump to Area 3 boundary • 8" water main along entire length of Areas 3a and 3b in internal local road from 14" water main in #3b to Gilead Lane • 24" water main extension from Gilead Lane to Area 3a local road
Sewer
<ul style="list-style-type: none"> • 8" sewer main from middle of Area 3a local road to 8" sewer main in Gilead Lane • Construct Sewage lift station and 4" sewer force main from middle of Area 3b local road to Golden Hill Road (north) 8" sewer main • 8" sewer main within west end of Gilead Lane to existing sewer main

Table 2-9. Subarea 4 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Union Road widening to 4-lane arterial standard • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basin 3B, or other detention basins designed to mitigate post-development flows as applicable.
Water
<ul style="list-style-type: none"> • None
Sewer
<ul style="list-style-type: none"> • 8" sewer main from west Area 4 to Union Road to Union Road lift station • Construct lift station on Union Road • 6" force main from Union Road lift station to existing 15" Highway 46 8" Union Road sewer main

Table 2-10. Subarea 5 Onsite Infrastructure Requirements
(only applicable if City Fire Station is located in Subarea 5)

Traffic and Circulation
<ul style="list-style-type: none"> • Airport Road half width improvements only as required for access to the proposed Fire Station • Union Road improvements adjacent to Subarea 5
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins 8A , or other detention basins designed to mitigate post-development flows as applicable, being sure to include all detention required downstream of developed areas within subarea 5.
Water
<ul style="list-style-type: none"> • 24" water main extension from Gilead Lane to Airport Road • 24" water main from Gilead Lane to Union Road via Airport Road • 12" water main extension from Union Road to Airport Road
Sewer
<ul style="list-style-type: none"> • 8" sewer main from southwest corner of Area 5 to Union Road • 12" sewer main in union road to proposed lift station • Construct lift station on Union Road • 6" force main from Union Road lift station to existing 15" Highway 46 sewer main • 8" sewer main in Gilead Lane (between areas 6 and 7) • 8" sewer main within west end of Gilead Lane to existing sewer main



Table 2-11. Subarea 6 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Extension of Gilead Lane to Airport Road • Airport Road from Gilead Lane to Union Road • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins 8A, 8B, and 8C, or other detention basins designed to mitigate post-development flows as applicable.
Water
<ul style="list-style-type: none"> • 24" water main extension from Gilead Lane to Airport Road • 24" water main from Gilead Lane to Union Road via Airport Road • 12" water main extension from Union Road to Airport Road
Sewer
<ul style="list-style-type: none"> • 8" sewer main from southwest corner of Area 5 to Union Road • 12" sewer main in union road to proposed lift station • Construct lift station on Union Road • 6" force main from Union Road lift station to existing 15" Highway 46 sewer main • 8" sewer main in Gilead Lane (between areas 6 and 7) • 8" sewer main within west end of Gilead Lane to existing sewer main

Table 2-12. Subarea 7 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Airport Road from Linne Road to Union Road, and Gilead Lane from Airport Road to area 7 local road, provided there are two points of vehicular access from major roadways to development (Option A) <u>OR</u> • Extension of Gilead Lane to Airport Road and extend Airport Road from Gilead Lane to Linne Road (Option B) • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basin 8B, or other detention basins designed to mitigate post-development flows as applicable.
Water
<ul style="list-style-type: none"> • Construction of (OBWZ) booster pump near Golden Hill Road • 14" water main from (OBWZ) booster pump to 14"-12" transition point. • 12" water main (OBWZ) from 12"-14" pipe transition to Area 7 , continuing to Gilead lane • 24" water main (MEWZ) in Gilead lane from area 7 local road to Airport Road Including PRV • 12" water main (MEWZ) from existing Linne Road 12" water main to transition to 16" main (north) on Airport Road via Airport Road • 16" water main from terminus described above to Airport Road/Gilead Lane intersection • 24" water main from Airport Road/Gilead Lane intersection to Union Road via Airport Road
Sewer
<ul style="list-style-type: none"> • 8" sewer main from north Area 7 to Gilead Lane • 8" sewer main in Gilead lane westerly end of area 6. • 8" sewer main from Gilead Lane to Union Road • 12" sewer main in Union Road to proposed lift station. • Construct Union Road lift station • 6" force main from Union Road lift station to existing 8" Union Road sewer main • 8" sewer main from area 9 on Airport Road to Linne Road (if Traffic & Circulation Option A is used) • 8" sewer main within west end of Gilead Lane to existing sewer main (if Traffic & Circulation Option B is used)



Table 2-13. Subarea 8 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Area 8 local road loop from Shadow Canyon (East Tract 2281) to Sherwood Road. • Alternate (Optional) second access point, to Airport Road. • Extension of Sherwood Road to area 8 local road • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basin 13 and 14A.
Water
<ul style="list-style-type: none"> • Construction of (OBWZ) booster pump near Golden Hill Road • 14" water main from (OBWZ) booster pump to 14"-12" transition point. • 12" water main (OBWZ) from 12"-14" pipe transition to existing east Tract 2281 8" main via Area 1 road • 12" water main (OBWZ) from area 1 through area 8 to Sherwood Road, including New PRV Valve. • 16" water main (MEWZ) extension from existing Sherwood Road 16" water main to 12" water main at Sherwood Road/ Area 12 internal road intersection • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 8" sewer main in Area 8 to Sherwood Road via local road • 8" sewer main in Area 8 to Sherwood Road via easement along westerly edge of tract • 8" sewer main in Sherwood Road west to existing 8" sewer main.

Table 2-14. Subarea 9 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Airport Road from Linne Road to Union Road (Option A) <u>OR</u> • Extension of Gilead Lane to Airport Road and extend Airport Road from Gilead Lane to Linne Road (Option B) • Additional roadway connection to Airport Road to ensure two points of public access to development • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins 8B and 14A
Water
<ul style="list-style-type: none"> • Construction of (OBWZ) booster pump near Golden Hill Road • 14" water main from (OBWZ) booster pump to 14"-12" transition point on Area 1 internal road • 12" water main (OBWZ) from 12"-14" pipe transition to Area 9 via Area 1,7,an 8 internal local roads, including portion of OBWZ 12" main in Airport Road extending to area 7 • 12" water main (MEWZ) from existing Linne Road 12" water main to transition to 16" main (north, adjacent to area 7) on Airport Road via Airport Road • 16" water main from terminus described above to Airport Road/Gilead Lane intersection • 24" water main from Airport Road/Gilead Lane intersection to Union Road via Airport Road • 12" water main (MEWZ) from Airport Road (south Area 9) looping back to through area 8 to and including new PRV • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 8" sewer main from area 9 on Airport Road to Linne Road • 10" sewer main from intersection of Sherwood and Airport Road south to Linne road, west to Fontana Road and south to Existing 10" on Fontana Road • 8" sewer main in Gilead lane from area 7 to westerly end of area 6. (if Traffic & Circulation Option B is used) • 8" sewer main within west end of Gilead Lane to existing sewer main (if Traffic & Circulation Option B is used)



Table 2-15. Subarea 10 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Extension of Gilead Lane to Airport Road • Airport Road from Gilead Lane to Union Road • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basin 6, or other detention basins designed to mitigate post-development flows as applicable, being sure to include all detention required downstream of developed areas within subarea 10.
Water
<ul style="list-style-type: none"> • 24" water main extension from Gilead Lane to Airport Road • 24" water main from Gilead Lane to Union Road via Airport Road including PRV • 12" water main extension from Union Road to Airport Road
Sewer
<ul style="list-style-type: none"> • 8" sewer main in westerly portion of Gilead Lane to existing Gilead Lane 8" sewer main • Construct lift station on Union Road • 4" force main from Union Road lift station to existing 8" Union Road sewer main • 8" sewer main from west Area 10 to Union Road to Union Road lift station • 8" Sewer in Gilead Lane adjacent to area 6

Table 2-16. Subarea 11 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Extension of road(s) from existing tract to the east to Area 8 local road • Area 8 Local road from Area 11 south to Sherwood Road • Sherwood Road from existing to area 8 local road • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins 14A
Water
<ul style="list-style-type: none"> • 16" water main (MEWZ) extension from existing Sherwood Road 16" water main to planned 12" main • 12" water main at Sherwood Road extension north on Area 8 internal local road to and including PRV • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 8" sewer main in Area 8 to Sherwood Road via local road • 8" sewer main in Area 8 to Sherwood Road via easement along westerly edge of tract • 8" sewer main in Sherwood Road west to existing 8" sewer main.

Table 2-17. Subarea 12 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Airport Road from Linne Road to northern boundary of Area 12 • Realignment of Sherwood/Linne, with extension of Sherwood Road to Airport Road alignment • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins 14C
Water
<ul style="list-style-type: none"> • 16" water main extension in Sherwood Road to Airport Road • 12" water main (MEWZ) loop from intersection of Sherwood Road and Airport Road north to area 9, west to area 8 and south to Sherwood Road, including PRV • 12" water main from intersection of Sherwood Road and Airport Road to existing Linne Road 12" water main • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 8" main to connect to existing sewer infrastructure within Sherwood Road



Table 2-18. Subarea 13 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Realignment of Sherwood/Linne, with extension of Sherwood Road to Airport Road alignment • Airport Road from Linne Road to Sherwood Road extension • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basin 14B
Water
<ul style="list-style-type: none"> • 16" water main extension from Sherwood Road to Airport Road • 12" water main from Sherwood Road extension/Airport Road intersection to existing Linne Road 12" main • Possible construction of water supply wells depending on final siting • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 10" sewer main from intersection of Sherwood and Airport Road south to Linne road, west to Fontana Road and south to Existing 10" on Fontana Road • 8" sewer main in Sherwood Road westerly to connect to existing infrastructure

Table 2-19. Subarea 14 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Airport Road from Linne Road to northern boundary of Area 14 • Realignment of Sherwood/Linne, with extension of Sherwood Road to Airport Road alignment • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins 14D, 15A
Water
<ul style="list-style-type: none"> • 16" water main extension in Sherwood Road to Airport Road • 12" water main from north boundary of Area 14 to existing Linne Road 12" water main via Airport Road • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 8" sewer main within Airport Road adjacent to area 14 to intersection of Sherwood Road and Airport Road • 10" sewer main from intersection of Sherwood and Airport Road south to Linne road, west to Fontana Road and south to Existing 10" on Fontana Road • 8" sewer main in Sherwood Road westerly to connect to existing infrastructure

Table 2-20. Subarea 15 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Realignment of Sherwood/Linne, with extension of Sherwood Road to Airport Road alignment including Airport Road/Airport Road intersection south to existing Linne Road • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of an appropriate detention basin
Water
<ul style="list-style-type: none"> • 16" water main extension in Sherwood Road to Airport Road • 12" water main from north boundary of Area 14 to existing Linne Road 12" water main via Airport Road • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 8" sewer main within Airport Road adjacent to area 14 to intersection of Sherwood Road and Airport Road • 10" sewer main from intersection of Sherwood and Airport Road south to Linne road, west to Fontana Road and south to Existing 10" on Fontana Road



Table 2-21. Subarea 16 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Realignment of Sherwood/Linne, with extension of Sherwood Road to eastern end of Area 16 dedicated and developed to 4-lane arterial standards and connected to the existing Linne Road in a manner to be approved by the City Engineer • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basin 15b
Water
<ul style="list-style-type: none"> • 16" water main extension from Sherwood Road to Linne Road (south of Area 16) • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Basin
Sewer
<ul style="list-style-type: none"> • 8" sewer main within Airport Road adjacent to area 14 to intersection of Sherwood Road and Airport Road • 10" sewer main from intersection of Sherwood and Airport Road south to Linne road, west to Fontana Road and south to Existing 10" on Fontana Road

Table 2-22. Subarea 17 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Realignment of Sherwood/Linne, with extension of Sherwood Road from Airport Road to eastern end of Area 17 dedicated and developed to 4-lane arterial standards and connected to the existing Linne Road in a manner to be approved by the City Engineer. • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basin 15C, or other detention basins designed to mitigate post-development flows as applicable.
Water
<ul style="list-style-type: none"> • 16" water main extension from Sherwood Road to Linne Road (south of Area 16) • Implement the appropriate 1993 Water Master Plan provisions
Sewer
<ul style="list-style-type: none"> • 8" sewer main within Airport Road adjacent to area 14 to intersection of Sherwood Road and Airport Road • 10" sewer main from intersection of Sherwood and Airport Road south to Linne road, west to Fontana Road and south to Existing 10" on Fontana Road • Implement 1993 Water Master Plan provisions pertaining to the Meadowlark Sewer Basin, or latest Master Plan if superseded

Table 2-23. Subarea 18 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Airport Road from Union Road to Hwy 46, including an arterial standard bridge over the Huerhuero Creek (18B if determined needed for access) • Road Improvements on Union Road Fronting the property (18A) • Right-of-Way for future interchange at Highway 46 and Airport Road (18B as applicable) • Access to Airport Road from other properties north of Huerhuero Creek (18B only) • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins as required
Water
<ul style="list-style-type: none"> • 24" water main from Union Road to Hwy 46 via Airport Road as applicable (18B) • 12" water main extension from Union Road to Airport Road (18A)
Sewer
<ul style="list-style-type: none"> • 8" sewer main from Union Road (south of Area 18A) to proposed Union Road lift station • Construct lift station on Union Road (18A) • 4" force main from Union Road lift station to existing 8" Union Road sewer main (18A) • For area 18B, sewer infrastructure as required and approved by the City



Table 2-24. Subarea 19 Onsite Infrastructure Requirements

Traffic and Circulation
<ul style="list-style-type: none"> • Airport Road from Union Road to Hwy 46, including an arterial standard bridge over the Huerhuero Creek(19B & 19C) • Easement for access Airport Road from 18B & 20C as applicable (19B & 19C). • Right-of-Way for future interchange at Highway 46 and Airport Road (19B & 19C as applicable) • Road Improvements on Union Road Fronting the property (19A) • All underground utilities within any new road improvements
Drainage
<ul style="list-style-type: none"> • Construction of Detention Basins as required
Water
<ul style="list-style-type: none"> • 24" water main from Union Road to Hwy 46 via Airport Road, or portions thereof as applicable • 12" water main extension from Union Road to Airport Road (19A)
Sewer
<ul style="list-style-type: none"> • 8" sewer main from Union Road (south of Area 19) to proposed Union Road lift station • 8" sewer main from Area 19 to Union Road 8" sewer main in Item #1 • Construct lift station on Union Road • 4" force main from Union Road lift station to existing 8" Union Road sewer main

Table 2-3 (on page 2-17) also shows the onsite and offsite circulation improvements needed, with the timing associated with such improvements. The discussion below clarifies the thresholds used to determine the timing of the improvements shown in Table 2-3.

Concurrent with Initial Development of CRASP Area.

Based on existing traffic conditions, the following locations will require improvements with initial development of the CRASP project area as current Levels of Service are at or below acceptable levels:

- State Route 46 East/US 101 NB Ramps
- State Route 46 East/US 101 SB Ramps
- Union Road/SR 46 East
- Union Road/Golden Hill Road

For SR 46E/US 101 SB Ramp intersection, an interim improvement is being proposed by the City of Paso Robles and Caltrans to add dual left turn lanes in the westbound direction. With this improvement, an additional westbound through lane will be added at the SR 46E/US 101 NB Ramp intersection, which will add sufficient capacity to improve Level of Service at both of these intersections. Due to funding limitations on constructing the improvements, it is not anticipated to be operational until 2015. As CRASP develops, the project will contribute its fair share to the mitigation. However, the near term LOS will degrade until the improvement is complete and operational.

Level of Service and safety problems are projected for mitigation to acceptable levels at the Union Road/SR 46E intersection with the prohibition of left turn movements from the northbound approach. This improvement would also relieve future projected Level of Service problems at the Union Road/Union Road Extension intersection.

At the Union Road/Golden Hill Road intersection, signalization and some additional widening or construction of a roundabout would provide interim traffic congestion relief and improve Levels of Service to acceptable conditions.



Area South of Gilead Lane.

Although the development timing of both residential and commercial development south of Gilead is not known, a threshold of no more than 500 single and multi-family dwelling units should be developed without improvement, consistent with the identified mitigation measures, to the following intersections.

- Golden Hill Road/SR 46E
- Rolling Hills Road/Golden Hill Road
- Niblick Road/Creston Road

With improvement to these intersections, the balance of the residential uses along with the commercial uses could be constructed.

Area North of Gilead Lane.

Commercial development of the CRASP area north of Gilead Lane is limited, particularly if access to SR 46E is restricted to a right turn movement only at the Union Road/SR 46E intersection (i.e. no northbound left-turn movements are permitted). Recognizing the limited access at the site from the highway, further improvements, as follows in the subsequent paragraph, should occur at the Golden Hill Road/SR 46E intersection and at the Airport Road/SR 46E connection before full development of the commercial uses is allowed north of Gilead Lane.

Golden Hill Road is currently operating at LOS "D" during the weekday PM peak hour period, on the cusp of LOS "C/D" (35.0s). With the completion of ongoing construction at the intersection (e.g. gas stations), the intersection is projected to operate at LOS "E". Assuming that SR 46E remains a four-lane divided arterial through its intersections with Golden Hill Road and that residential development occurs before all commercial development, the following intersection configuration at SR 46E/Golden Hill Road would allow for 1,200 residential units (80% of total) within CRASP to be accommodated at acceptable LOS:

- Northbound and southbound Golden Hill Road - one lane for each turn movement (left, through, right)
- Eastbound and westbound SR 46E - one left-turn lane, two through-lanes, one right-turn lane.
- Signalize all approaches with protected phasing.

With the same intersection configuration, 1,050 residential units (72% of total) within CRASP could be accommodated concurrently with the full development of the CRASP commercial area fronting Niblick/Sherwood Road (equivalent to 150 PM peak hour trips) and development equivalent to 200 PM peak hour trips on the CRASP commercial area bounded by Union Road and Huerhuero Creek. As previously stated, the development of the CRASP commercial area bounded by SR 46E and Huerhuero Creek would not be possible without an Airport Road/SR 46E connection.

c. Implementation and Financing. Action Item 3 of Policy LU-4a of the adopted City of Paso Robles General Plan requires that

"...a fiscal impact analysis for new development in annexation areas and/or specific plan areas and condition projects accordingly so as to ensure that they will be fiscally neutral and not result in a net loss for the City."



The General Plan calls for new development in the Chandler Ranch Area to “pay its own way.” Accordingly, the cost of any public improvements and infrastructure not constructed directly by Plan Area developers must be distributed among the development anticipated in the area in the form of fees proportionate to benefited parcels and projects or as conditions on discretionary approvals.

Costs associated with development under the Specific Plan are broadly divided into

- Capital Improvements; and
- Operations and Maintenance.

Capital improvements refer to major infrastructure, including roadways, bridges, water, sewer and drainage facilities needed to serve new development. Operation and Maintenance refers to the long-term costs of operating these facilities, as well as other facilities within the Plan area such as public landscaping and lighting. The financing of each is discussed below.

Capital Improvements Costs. There are five methods that would be used to fund required capital improvements and City services under the Chandler Ranch Area Specific Plan:

1. **Development Fees.** Standard City development fees to implement citywide improvements, per AB 1600;
2. **Developer Responsibility for Onsite Improvements.** Direct responsibility of the developer to construct the improvements identified in the Specific Plan for a particular subarea prior to development within that subarea. (The specific timing of needed improvements is noted in the “Onsite Infrastructure Requirements” table associated with each subarea, as included in Section 3.0 of this specific plan.) On-site improvements, including construction of adjacent roads and traffic improvements, shall be constructed or guaranteed in a form to be approved by the City Engineer, prior to recordation of a final map for development within the applicable sub-area. Construction shall be complete prior to issuance of Certificates of Occupancy. It should be noted that construction of Airport Road south of Union Road, and Sherwood Road from Fontana Road to Linne Road, would be the financial responsibility of certain property owners within the Plan Area pursuant to the Annexation Agreement of 1980 (included in the Specific Plan as Appendix C), and the financial obligation to construct this facility would be subject to the requirements of this agreement, as described in this Specific Plan;
3. **Specific Plan Fees for Major Off-Site Circulation Improvements.** Specific Plan fees payable to the City for the purpose of constructing certain other major off-site circulation improvements of primary benefit to developments within the specific plan area and/or that would not be needed except for development under the Specific Plan, with fees based on a per dwelling unit equivalency for development under the Plan.
4. **Community Facilities District Fees.** Consistent with “fiscal neutrality” called for in the City’s General Plan, it is anticipated that a Community Facilities District (CFD) will be formed and will serve to fund three areas of impact: (a) incremental demand



for City services; (b) a share of mitigation of impacts on the Paso Robles School District; and (c) City infrastructure proposed by the property owner to be financed through the City's Community Facilities District.

5. **Property Owner Participation in City Capital Projects.** Certain capital facilities will be needed to serve new development within the Chandler Ranch Area Specific Plan. These projects will be either constructed by the City or, subject to mutual agreement, the City Council may permit the facilities to be constructed by developers of property within the Chandler Ranch Area and provide proportionate City reimbursement.

Specific Plan Area property owners will be charged their proportionate share of the cost of providing these facilities, including the cost of land (reimbursing property owners for facilities that serve more than their property), project design and administration, financing (if applicable), and capital construction costs. These capital projects include but are not limited to proportionate shares of the following:

- a. Water production wells or equivalent Nacimiento water;
- b. Water storage tanks or equivalent value in land for tank sites;
- c. Emergency Services Facility / Fire Station;
- d. Traffic circulation and control facilities, including any traffic calming facilities adjacent to or within the specific plan area.

Capital facilities needs will be identified during the specific plan process and confirmed prior to granting of entitlements for development for the respective sub-areas. The City Council will review and approve allocated proportionate shares and establish any applicable reimbursement provisions prior to granting of entitlements for development.

The application of each mechanism as it relates to Specific Plan development is included in more detail within the Specific Plan document.

Operations and Maintenance. Most of the City's operational costs of serving new residential development would be covered through the previously described CFD. The exception would be funding for Landscaping and Lighting, and the potential for funding public street maintenance.

The City has established already assessment districts for maintenance of City street lighting and landscaping. This is the preferred method to be used in the Chandler Ranch Specific Plan area for ongoing maintenance of these types of improvements. The City also has the option to require street maintenance districts to be formed. To date, the City has not implemented this type of district.

Any type of Assessment District would require voter approval of the majority of affected voters within the District boundaries.

To insure that necessary services are maintained even if an established assessment district does not remain in place for the lifetime of the Chandler Ranch development, a Homeowner's



Association will be required to be created to fund the necessary operations and maintenance of these facilities in case the CFD and/or Landscaping and Lighting District are dissolved.

Improvements Within Specific Plan Developments. Developers will be required to fully construct the following improvements within development projects under the Specific Plan:

- Arterial, collector, and local roads, sidewalks and landscaping;
- Utilities to individual properties and to project boundaries;
- Grading and drainage for individual properties;
- Street lighting;
- Other public improvements as specified in the City Standards and as required by the City Engineer.

2.5 PROJECT OBJECTIVES

The State CEQA Guidelines requires that the EIR Project Description include "a statement of objectives sought by the proposed project." The City's General Plan has established a Specific Plan requirement for the Chandler Ranch area. The intent is to provide a cohesive planning framework, such that the major land use, circulation, and infrastructure requirements can be better coordinated and more logically planned.

As described in the General Plan Land Use Element, the purpose of the Specific Plan Overlay Designation that applies to the Chandler Ranch site is as follows:

Specific Plan (SP)

Purpose: This overlay designation is established where infrastructure needs, land use patterns, or other substantial land use related issues indicate a need to require the preparation and adoption of a Specific Plan, as defined by California Government Code sections 65450 et seq. In such instances, the City may require completion of a specific plan prior to approval of a subdivision or development plan for any property located within the Specific Plan category. The City Council will determine the method of funding for a specific plan on a case by case basis.

Within specific plan areas, a fee schedule may be established to provide adequate funding for on- and off-site public facilities and improvements of benefit to properties within the designation specific plan areas. Such fees are above and beyond any property-specific or City-wide property taxes, fees, charges, or assessments.

2.6 REQUIRED APPROVALS

Implementation of the proposed project would require the following legislative and discretionary approvals from the City of Paso Robles or other responsible agencies:

- *Certification of the Chandler Ranch Area Specific Plan Final EIR by the City of Paso Robles;*



- *Approval of a General Plan Amendment reflecting entitlements proposed under the Chandler Ranch Area Specific Plan;*
- *Establishment of "Gateway Commercial Zoning Overlay;*
- *Adoption of the Chandler Ranch Area Specific Plan by the City of Paso Robles.*

After Specific Plan adoption, but prior to development under the Specific Plan:

- *Individual property owners subdivision of property to be approved by the City of Paso Robles;*
- *Architectural Review of proposed building and site improvements by the City of Paso Robles;*
- *Building permit approval by the City of Paso Robles;*
- *Section 404 permit for any development with impacts to Federal waters under the jurisdiction of the Army Corps of Engineers;*
- *Section 401 permit for any development with impacts to State waters under the jurisdiction of the Regional Water Quality Control Board;*
- *Section 1602 Streambed Alteration Agreement for development impacts to streambeds under the jurisdiction of the California State Department of Fish and Game;*
- *Compliance with Kit Fox Habitat mitigation and other requirements of the U.S. Fish and Wildlife Service and/or the California Department of Fish and Game;*
- *Zone changes to be consistent with the City's General Plan;*
- *Development Agreements between the City and property owners; and*
- *Use of Public Financing, subject to City Council approval*

The above list includes approvals known to be required for one or more components of the proposed Specific Plan. Other approvals may be required as individual future projects are proposed for the project area. This EIR is intended to facilitate adoption of the Chandler Ranch Area Specific Plan.

Subsequent CEQA Review of Development Consistent with the Specific Plan. Section 65457 of the California Government Code provides that once the EIR has been certified and the Chandler Ranch Area Specific Plan adopted, any residential development project, including any subdivision or zone change, which is undertaken to implement and is consistent with the specific plan is exempt from additional CEQA review. This exemption does not apply if after the adoption of the specific plan, any of the events which would trigger preparation of a subsequent or supplemental EIR occur, including substantial changes in the project or circumstances under which the project is being undertaken requiring major revisions in the project, or new information becomes available which was not known at the time the EIR was certified. However, if a supplemental EIR is prepared covering the changes, new circumstances, or new information and is certified, the exemption will apply to the projects which then follow the specific plan.

